

## CITY OF WINTERS PLANNING COMMISSION AGENDA

**Tuesday, October 30, 2012 @ 6:30 PM**  
City of Winters Council Chambers  
318 First Street  
Winters, CA 95694-1923  
Community Development Department  
Contact Phone Number (530) 795-4910 #111  
Email: [maryjo.rodolfa@cityofwinters.org](mailto:maryjo.rodolfa@cityofwinters.org)

Chairman: Bill Biasi  
Vice Chairman: Pierre Neu  
Commissioners: Lisa Baker, Bruce  
Guelden, Richard Kleeberg, Luis  
Reyes, Joe Tramontana  
City Manager: John W. Donlevy, Jr.  
Mgmt. Analyst: Mary Jo Rodolfa  
Planner: Jim Bermudez

### **I CALL TO ORDER**

### **II ROLL CALL & PLEDGE OF ALLEGIANCE**

**III CITIZEN INPUT:** Individuals or groups may address the Planning Commission on items which are not on the Agenda and which are within the jurisdiction of the Planning Commission. **NOTICE TO SPEAKERS:** Speaker cards are located on the first table by the main entrance; please complete a speaker's card and give it to the Planning Secretary at the beginning of the meeting. The Commission may impose time limits.

### **IV CONSENT ITEM**

Approval of Minutes from the August 25, 2012 regular meeting of the Planning Commission

### **V STAFF/COMMISSION REPORTS**

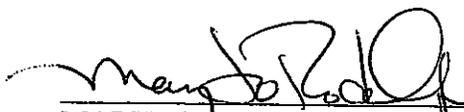
### **VI DISCUSSION ITEMS:**

- A. Public Hearing and Consideration of Approval of Conditional Use Permit and Design Review for proposed St. Anthony Church located at 511 Main Street
- B. Approval of Alley Activation Visioning Plan – an addendum to the Downtown Master Plan

### **VII COMMISSION/STAFF COMMENTS**

### **VIII ADJOURNMENT**

**POSTING OF AGENDA:** PURSUANT TO GOVERNMENT CODE § 54954.2, THE COMMUNITY DEVELOPMENT MANAGEMENT ANALYST POSTED THE AGENDA FOR THIS MEETING ON OCTOBER 25, 2012.

  
\_\_\_\_\_  
MARY JO RODOLFA, MANAGEMENT ANALYST

**APPEALS:** ANY PERSON DISSATISFIED WITH THE DECISION OF THE PLANNING COMMISSION MAY APPEAL THIS DECISION BY FILING A WRITTEN NOTICE OF APPEAL WITH THE CITY CLERK, NO LATER THAN TEN (10) CALENDAR DAYS AFTER THE DAY ON WHICH THE DECISION IS MADE.

PURSUANT TO SECTION 65009 (B) (2), OF THE STATE GOVERNMENT CODE "IF YOU CHALLENGE ANY OF THE ABOVE PROJECTS IN COURT, YOU MAY BE LIMITED TO RAISING ONLY THOSE ISSUES YOU OR SOMEONE ELSE RAISED AT THE PUBLIC HEARING(S) DESCRIBED IN THIS NOTICE, OR IN WRITTEN CORRESPONDENCE DELIVERED TO THE CITY PLANNING COMMISSION AT, OR PRIOR TO, THIS PUBLIC HEARING".

**MINUTES:** THE CITY DOES NOT TRANSCRIBE ITS PROCEEDINGS. ANYONE WHO DESIRES A VERBATIM RECORD OF THIS MEETING SHOULD ARRANGE FOR ATTENDANCE BY A COURT REPORTER OR FOR OTHER ACCEPTABLE MEANS OF RECORDATION. SUCH ARRANGEMENTS WILL BE AT THE SOLE EXPENSE OF THE INDIVIDUAL REQUESTING THE RECORDATION.

**PUBLIC REVIEW OF AGENDA, AGENDA REPORTS, AND MATERIALS:** PRIOR TO THE PLANNING COMMISSION MEETINGS, COPIES OF THE AGENDA, AGENDA REPORTS, AND OTHER MATERIAL ARE AVAILABLE DURING NORMAL WORKING HOURS FOR PUBLIC REVIEW AT THE COMMUNITY DEVELOPMENT DEPARTMENT. IN ADDITION, A LIMITED SUPPLY OF COPIES OF THE AGENDA WILL BE AVAILABLE FOR THE PUBLIC AT THE MEETING. COPIES OF AGENDA, REPORTS AND OTHER MATERIAL WILL BE PROVIDED UPON REQUEST SUBMITTED TO THE COMMUNITY DEVELOPMENT DEPARTMENT. A COPY FEE OF 25 CENTS PER PAGE WILL BE CHARGED.

ANY MEMBER OF THE PUBLIC MAY SUBMIT A WRITTEN REQUEST FOR A COPY OF PLANNING COMMISSION AGENDAS TO BE MAILED TO THEM. REQUESTS MUST BE ACCOMPANIED BY A CHECK IN THE AMOUNT OF \$25.00 FOR A SINGLE PACKET AND \$250.00 FOR A YEARLY SUBSCRIPTION.

**OPPORTUNITY TO SPEAK, AGENDA ITEMS:** THE PLANNING COMMISSION WILL PROVIDE AN OPPORTUNITY FOR MEMBERS OF THE PUBLIC TO ADDRESS THE COMMISSION ON ITEMS OF BUSINESS ON THE AGENDA; HOWEVER, TIME LIMITS MAY BE IMPOSED AS PROVIDED FOR UNDER THE ADOPTED RULES OF CONDUCT OF PLANNING COMMISSION MEETINGS.

**REVIEW OF TAPE RECORDING OF MEETING:** PLANNING COMMISSION MEETINGS ARE AUDIO TAPE RECORDED. TAPE RECORDINGS ARE AVAILABLE FOR PUBLIC REVIEW AT THE COMMUNITY DEVELOPMENT DEPARTMENT FOR 30 DAYS AFTER THE MEETING.

**THE COUNCIL CHAMBER IS WHEELCHAIR ACCESSIBLE**

# MINUTES OF THE WINTERS PLANNING COMMISSION MEETING HELD August 25, 2012

*DISCLAIMER: These minutes represent the interpretation of statements made and questions raised by participants in the meeting. They are not presented as verbatim transcriptions of the statements and questions, but as summaries of the point of the statement or question as understood by the note taker.*

Chair Biasi called the meeting to order at 6:30 p.m.

**PRESENT:** Chair Biasi, Commissioners Baker (arrived at 6:36), Guelden, Kleeberg, Reyes, Tramontana

**ABSENT:** Commissioner Neu

**STAFF:** City Manager Donlevy, Environmental Services Manager Scianna, Public Works Associate Landes and Management Analyst Rodolfa

**CONSULTANT:** John Mott Smith, Yolo County Climate Change Advisor

Commissioner Reyes led the Pledge of Allegiance.

**CITIZEN INPUT:** None

**CONSENT ITEM:**

1. Approval of Meeting Minutes of the July 24, 2012 regular meeting of the Planning Commission.

Commissioner Tramontana moved to approve the Meeting Minutes of the July 24, 2012 Planning Commission Meeting. The motion was seconded by Commissioner Kleeberg and passed unanimously.

**DISCUSSION ITEMS:**

**A. Climate Action Plan Workshop** – Carol Scianna, Environmental Services Manager presented a report on the Climate Action Plan Update. Scianna explained that the Climate Action Plan (CAP) is a planning document that provides strategies for reducing energy use and green house gas (GHG) emissions. The CAP offers a menu of GHG mitigation options that may be incorporated into projects allowing for streamlining planning review and providing the business community with a greater level of certainty about planning and permitting requirements. Scianna commented that although CAP is an important component for considering sustainability for municipal planning the City alone did not have the staff and budget to take on the project. In order to put this together the City turned to PG&E and the Yolo Energy Watch program. PG&E through the Yolo Energy Watch program contracted with a UC Davis professor to conduct the technical analysis and documentation for the Woodland and Winters CAPS.

Scianna reported that the enactment of AB32 (Global Warming Solutions Act) in 2006 established a statewide target for reducing GHG emissions to 1990 levels by 2020. Additionally the Governor signed Executive Order S-3-05 requiring reductions of 80% below 1990 levels by 2050. The Scoping Plan for implementation of AB 32 states that local governments should contribute to the state effort by reducing overall energy use by 15% by 2020 and take longer-term actions to meet the 2050 target. Additionally, in 2007 the state enacted SB 97 requiring amendments to the California Environmental Quality Act (CEQA) Guidelines to address the analysis and mitigation of GHG emissions. The City Council in recognizing the responsibility to meet State GHG reduction targets committed to joining the Yolo County Climate Change Compact. The Compact entered into a local government partnership with PG&E to form Yolo Energy



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Watch. It is through this program that UCD Professor and climate change and planning authority, Deb Niemeier was hired to engage a student group to prepare the CAPs for Woodland and Winters.

Scianna outlined the efforts the City has taken to date to reduce its GHG footprint including: participating in the climate registry, completing the municipal GHG inventory for 2008 and 2009, energy upgrades to City facilities, solar array, and promoting energy saving programs for residential, commercial and industrial users in the City. Planning items that have been implemented include SACOG Blueprint, Form Based Code, Caltrans Complete Streets, Energy Star Standards, Roundabouts planning and updating the bike and pedestrian master plan. Additionally the City has installed water meters contributing to water conservation, support of the Farm to School and Bike to School efforts in the community and economic development efforts to bring jobs to Winters.

Scianna then went through the 2005 Baseline Year Emissions Inventory power point presentation. She stated that of the six gasses listed the focus is on the three most important, trying to equalize the numbers. Winters On-Road Transportation within the city limits for the baseline report is 69%, Scianna stated that this is normal for a community like ours. City Manager Donlevy asked about our counting the trips going out of town. Yolo County Climate Change Advisor Mott-Smith stated that it is the destination that counts those trips and that the City is responsible only for what happens within its jurisdiction. Commissioner Kleeberg asked if SUVs were counted as passenger vehicles or light duty trucks. Mott-Smith said they are normally counted in the light duty truck category but he was not sure if that was the case in this report. City Manager Donlevy asked how the numbers were generated. Mott-Smith responded that they use SACOG data sources and DMV registration. Chair Biasi asked if 2005 data was used for all of it, Mott-Smith responded yes. In discussing GHG reduction for City government operations Scianna said that we use much less power now due to the solar array to power waste water treatment. She indicated that if we do nothing we won't meet the target, that if we utilize the new vehicle standards and building codes we will see a 7% reduction, almost halfway to the 15% target. The remaining strategies outlined above are to get us the rest of the way there. Scianna commented that the CAP is due to be completed by the end of the year and she will be coming back to the Planning Commission with more detailed information, indicating that this was just an overview of where we are so far and how we have gotten to this point.

Commissioner Guelden asked about the projection of the numbers into the future and if it was on a per household basis. Mott-Smith replied that it was on an aggregate basis. Guelden asked if that was reasonable and if the Planning Commission was being put in a position that a person can't even put in a lawn. Donlevy said that the Green Code already dictates some of that. Commissioner Baker stated that in Sacramento they have put in place an incentive program to replace lawns with vegetation meeting the Green Code. Scianna commented that the big issue will be transportation, how to get from Point A to Point B. Baker commented on the use of 2005 as the baseline but the need to get to 1990 emissions. Guelden said he does not believe it is a realistic regardless of the population, we can end up in the position where we won't want businesses to come to town if they are emitters of gas. Scianna and Public Works Associate Landes said it is more about transportation, that is the really big area. Kleeberg indicated that rather than worrying about 2050 we should worry more about 2015 or 2020 – that is more realistic. Donlevy stated that the math will be very calculated, that energy is produced differently now than in 1990, and that 1990 vehicle emissions are different than today. Guelden commented that the State comes up with numbers out of a hat and asked why would a business want to come to California, that PG&E costs will go up. Donlevy agreed, stating that Cap & Trade is a reallocation of wealth from one industry to another, it is a big complaint of new businesses. Biasi voiced concern about what will happen if we don't hit the targets, Baker stated that adoption of the CAP creates a safe harbor from lawsuits. Donlevy said staff is working on pragmatic solutions such as bikes to school.



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**B. Agricultural Industrial Aggregation Hub Update** – City Manager John Donlevy reported that the City has been working with Yolo County on the development of an Agricultural Industrial Aggregation Hub. He indicated that Commissioner Baker is a member of the working group. People are interested in the ability to produce and package agricultural products here. AB 32 also affects farmers and there is the pressure to produce more, water less and have higher efficiency. Trucking products runs into the cap and trade element. The idea is to find places in Winters agricultural/industrial zones. Donlevy displayed a bubble graph indicating the 9 components of the hub. In Winters we currently have processing and distribution but now we are beginning to also see value added and regulatory such as the Mariani food safety lab. The biggest benefactors will be small and medium sized farmers. At Mariani's 50% of staffing is going into value added with their nuts, but if you remove nuts from the equation most of what is grown here is not processed here, less than 90%. We are looking at private industry doing research on agricultural equipment, an agricultural business park with bottling, commercial kitchens, box making, distribution and brokerage marketing. Right now there are three areas being considered, all are outside the City General Plan area thus we are working closely with Yolo County and actively talking with property owners. The three main components right now are planning issues – finding land and zoning it as ag industrial specialty zone; infrastructure – what is needed to make this happen, how does it pencil out, what kind of water supply, waste water processing, etc.; and economics – transcend how it can work for developers/farmers - SACOG may have funding to help with this project. Currently working on an upfront analysis to look at the economics and feasibility then a lot of outreach will be done. This would come to the Planning Commission as expansion of the Sphere of Influence or City limit – annexation is a possibility. Currently they have a lead on two businesses, one is a meat processing business and the other is a major wine operation. This will most likely become a recurring item for the Planning Commission. Baker commented that USDA is very interested in this idea as is Capay Ag Tourism, Winters is seen as a natural partner. Biasi asked if any of this will fit in our current zoning, Donlevy responded yes but for some of the larger projects more land is needed. Donlevy commented that we are trying to create a corridor, become food central as our niche. The reality is we are agriculture. Biasi indicated that the proximity to I-505 bodes well and that he is in support of the idea.

## STAFF REPORT:

**A. Ring of Fire – Buckhorn** – John Donlevy, recommended that this item be moved down to the bottom of the agenda since John Pickerel of the Buckhorn would be late. The Commissioners agreed to move the item to the bottom of the agenda. John Pickerel representing Buckhorn Steakhouse addressed the Commission about a Brazilian BBQ (Ring of Fire) on the bulb out. Pickerel commented that the bulb out has been very successful for his business and he wants to leverage the phenomena and use Winters to leverage business. He would use social media to promote the experience of the Ring of Fire BBQ. He said using social media for promotion is most effective when you are virally communicating entertaining things and photos. The product would be street food - self contained, affordable and lending to photos. With the development of a downtown hotel it would be compatible to hotel guests and people attending conferences. Pickerel told the Commissioners that as this is public property you will get asked why the Buckhorn is allowed to do this, but if it works every other city will want one and it will be compelling street food. He indicated that he wants to begin Spring of next year. Fire is a component of what he is planning to do, it is their version of a churasco Brazilian bbq, meats on skewers cooked over fire with interesting small sides. It is designed to promote fun and conviviality. People can walk up and access it immediately with no hostess, it is a



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walk up bar type of thing. There is variety of food items, and the eating style is grazing. He wanted to make a play on the name, on the word fire, people are drawn to the idea of an open fire. Pickerel then drew the attention of the Commissioners to the drawing that had been provided in their packets saying that what they see is not complete. The actual area should be rounder and have a stronger element of fire. He said it is going through the vetting of the health department right now and is working on function more than form. Commissioner Tramontana likened it to a kind of benihana bar. He wanted to know if you walk up then where would you eat. Pickerel said there would be about 12 seats and that initially it would be only on Thursday nights. Thursdays are seen as transitional days prior to the weekend. The menu would be a traditional churasco style, a waiter comes by with the food, slices it off and serves. That is how it is done in Brazil, the server goes to the table with the skewers. Tramontana commented that it might be good to have a tall table where people can sit or stand around. Pickerel said most will be served at other tables on the patio besides what is depicted in the drawing. Pickerel said he does not want to disrupt the city's view of itself and that basically he is asking permission to build an attractive bonfire on the corner. What he wants to know of the Planning Commission at this point is if they are okay with him going forward and spending more money on plans. Is this something they can defend? Chair Biasi said you are already doing that. Pickerel responded that we have pulled this back so we can get proper planning. Commissioner Reyes asked will Putah Creek pizza oven and other things still be going on? Pickerel said, yes, they would have to be busy to make this work. This would add a different aspect to the restaurant, smaller plates, smaller prices, grazing. There is pressure to be more fun and entertaining, sell experience. Tramontana commented that it is his feeling the Buckhorn is known all over, that the City doing the bulb outs was a quaint thing and now this town is hopping for four nights a week. He believes that this will add to what people are looking at and it will be a big attraction. Pickerel said that if they can get the fire and sangria right it will work. Commissioner Kleeberg said he would like to encourage Pickerel to move ahead, on a trip to Paris years ago he and his wife discovered a Lebanese spot with hanging racks of meat over small flames, it was a wonderful meal. Since then he has discovered it in other areas of Paris and in Germany. In both places there were people having a good time and enjoying it. People smiling and talking. Sure there are obstacles but he would like to see Pickerel move ahead. Pickerel indicated that the Japanese also have a version of this and that there is an attraction to sitting around fires and conversing. The fire part will work on a visceral basis, just working on getting it to work on the corner and the design component. Pickerel said he will beat up the design a million times before he is done with it. Biasi said he likes the idea overall, using the bulb outs to bring people in, to get people coming together outside, however there is a problem with access being blocked. Access is a problem on the Putah Creek side. Biasi would also like to know how the fire department feels about it and suggested it move farther behind wall to the west. Tramontana mentioned that right now where sink is shown there is a garbage can. Biasi stated that this drawing does not show the details. Pickerel responded that those things would be looked at. Biasi wanted to know if there will there be a lower portion on the counter that is accessible? Pickerel said that he is a fan of lower tables and that he has not yet asked the fire department about this. Commissioner Baker said she wants to preserve access but she wants to also make sure it is okay with multi generational customers, there are problems with tables too low or too high. Biasi commented that as for the use of city property that is why we put the bulb outs in. Baker said that if we trying to attract folks then we need to have the experience piece, you need traffic if you want storefronts to stay open later. Commissioner Guelden commented that when the bulb outs went in there was an agreement for the use of the bulb outs and that the Buckhorn pays for the use of the land. He wanted to know if this could be looked at as just part of that agreement. City Manager Donlevy said yes it could, that the development of the plans it will be planned checked. It won't go



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until it passes muster but his opinion is it fits in with use. Guelden asked Pickerel if the bulb outs worked out well for him. Pickerel said yes, there is now a sense of arrival at the corner, you have arrived at the center of town, you find a place to park and get out. There is something about being outside. The car show has been so successful it wants to expand to twice a month. You add music and small town charm, people on the street, and if outdoor seating on Railroad happens then people will go down to Turkovich. Being able to sit outside is a money generator. Biasi commented that the pizza oven has brought a lot to downtown. On Thursday nights there are as many as 300 cars on a downtown and 400 on a Saturday night. Tramontana said wouldn't it be nice if we have so many cars that we don't have enough spaces. Pickerel said that what we are after is critical mass. If there is enough contributing to the vibrancy of the downtown that is unique to it and different than what is at the freeway then it will help with sustainability. The downtown property owners could then get better tenants renting space. He is currently working on trying to get UCD to bring busses to Winters, and we need a reason for them to come to Winters. The UC Davis marketing department is beginning to see Winters as a destination. Pickerel said he won't do this until it is a good idea and that he may look at relocating it. The Commissioners were in agreement with Pickerel moving forward with this idea. Donlevy asked the Commissioners if they wanted to take a field trip? The Commissioners responded they are good for now, Pickerel said he is not ready for it yet but that he will be later. He added if we get this right and it is the star attraction of Thursday nights which are typically the second or third day of meetings for conference folks. The personality of the town changes on Thursday night and other businesses will view Thursdays differently. He added that Yolo Prime Sundays, doubled his business by offering value meals at a different time of day.

## COMMISSIONER/STAFF COMMENTS:

Chair Biasi commented on the email update from Scianna regarding the Putah Creek Trail project. Scianna commented that the trail is delayed a bit because the construction crew took work elsewhere because of hawks that were nesting, they are gone now but the crews are committed elsewhere. She indicated that the deck is done but there is no access. The plan is still to have it completed on time and AT&T beginning their undergrounding work as part of the car bridge project. The car bridge project is going out to bid in September or October. There is about a 4 ½ month bid window, and in February or March we will open the bids, and construction will begin in June. The temporary bridge and trestle for the crane will be constructed the first year, the second year will be the demolition of the current bridge and construction of the piers, the third year will be construction of the new bridge and the fourth year will be the removal of the temporary bridge.

**ADJOURNMENT:** Hearing no objections Chairman Biasi adjourned the meeting at 8:22 p.m..

## ATTEST:

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Mary Jo Rodolfa, Management Analyst

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Bill Biasi, Chairman





**PLANNING COMMISSION  
STAFF REPORT**

**TO:** Honorable Chairman Biasi and Commission Members  
**DATE:** October 30, 2012  
**FROM:** Jim Bermudez, Contract Planner  
**SUBJECT:** Public Hearing regarding St. Anthony's Church Conditional Use Permit and Design Review located at 511 Main Street

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**RECOMMENDATION**

Subject to the attached conditions of approval, staff recommends that the Planning Commission take the following action:

1. Certify that the Planning Commission has determined that the previously certified Mitigated Negative Declaration is the appropriate level of environmental review and that none of the conditions described in Section 15162 of the CEQA Guidelines warranting new environmental review exists.
2. Approve a Conditional Use Permit to allow a religious institution in a residential zone.
3. Approve Design Review of site plan, building architectural design, architectural materials, architectural finishes, color schemes, and conceptual landscape plan for the church and associated site improvements.

**BACKGROUND**

The site development started in the late 1980's with the construction of the existing 13,235 square foot St. Anthony's Parish Community Center and associated parking areas and improvements on the north and north eastern portion of the property. A future church, rectory, and ancillary building were anticipated at that that time but were not developed.

In May 2008, the applicant submitted a conceptual design and site plan for a proposed 700 seat church and rectory. The proposed site plan and architectural style was presented to the Planning Commission for Conceptual Design Review on May 27, 2008.

On March 9, 2009 the applicant informed the City that the Parish had purchased the adjoining residence (507 Main Street) for use as a rectory and would not be proceeding with the proposed rectory on the project site in the foreseeable future. Due to the shortage of required on-site parking, staff requested the site plan be revised to include parking where the rectory was proposed to be located.

On March 24, 2009, the Planning Commission approved a Planned Unit Development Overlay (to modify parking and height limits), Conditional Use Permit (religious institution in an R-2 zone), and Design Review for a 13,813 square foot, 700 seat church and a future 2,900 square foot rectory with a two car garage.

On March 12, 2010, the City received a letter from St. Anthony's Church, requesting an

extension of time for the approved planning entitlements. Staff determined circumstances under which the entitlements were granted did not substantially change so the use permit was extended one year (maximum allowed per municipal code) to March 24, 2011 and the Design Review/Site Plan and Planned Unit Development Permit were extended two years to March 24, 2012 and April 21, 2012.

During the entitlement extension period, the City regularly communicated with the church, urging the church to submit building plans before the entitlement extension period ran its course. Based on discussions with the applicant, the church experienced a turnover of three pastors during the extension period and consequently key decisions on planning and design were either delayed or changed resulting in the use permit and Design Review/Site Plan entitlement extensions to expire. The City was able to preserve the Planning Commission's approval of the Planned Unit Development Permit which modifies height restrictions and parking requirements.

## **PROPOSED PROJECT**

This project proposes to construct a 10,814 square foot, 436 seat church at the southeast corner of Grant Avenue (State Route 128) and West Main Street (Vicinity Map, Attachment B).

The property is 5.56 acres and is currently developed with a 13,235 square foot community center, 827 square foot auxiliary building; a storage shed, and associated parking and landscaping. The parcel is generally square with frontage on Grant Avenue, and West Main Street. The frontage along Grant Avenue is 478 feet in length and the frontage on West Main Street is approximately 472 feet. The parcel abuts Waggoner Elementary School to the east and a residential neighborhood to the south (Site Plan, Attachment C).

The total on-site parking required for the site developed with the existing community center, existing auxiliary building and proposed church would be 242 spaces. The proposed project, as redesigned is proposed to provide 259 parking spaces.

*Proposed Building* – The church is an 10,814 square foot Spanish Colonial style building which would seat 436 and includes space for a lobby, acolytes-altar servers, vestry-dressing room, confessionals, restrooms, arts-eucharist preparation, storage, sacristy-religious storage, sanctuary-altar, choir, sound system, electrical-mechanical service, music room, janitorial, and bride's room (Preliminary Floor Plan, Attachment D). The building is single story and is located in the west central portion of the parcel. Due to the roofline, the building is 43' 10" tall with total height of 52' including the roof mounted cross (Building Elevations, Attachment E).

*Wastewater* - Sanitary sewer service for the church is proposed to be provided via an 8" main which would be constructed across the central portion of the site and would connect to an existing 8 inch municipal sanitary sewer line located at the western boundary of the property. The City's Wastewater Treatment Plant (WWTP) has a capacity of 0.92 million gallons per day (mgd). Space remains for approximately 600 additional residential hook-ups. The City's recent project approvals exceed this amount and expansion of the plant is planned. The Phase 2 expansion will bring the capacity to between 1.2 and 1.6 mgd.

*Water* - Municipal water is proposed to be provided to the property via the existing 12-inch water main on the north side of the property and the 10 inch water main on the west side of the property, and the 6 inch water main on the south side of the property. Water would be conveyed within the property via 8, 6, 4 and 3 inch lines which are proposed to connect to the municipal lines on the north and west south sides of the property.

*Stormwater* - Storm water is proposed to be collected on site via a series of grated intakes in parking and driveway areas and conveyed off site via an east-west running storm drainage line to the western edge of the property where it would connect to an existing 24 inch municipal storm water drain line located in west Main Street.

*Off-Site Infrastructure* - The project would be required to fund and construct off-site improvements necessary to support the development. Such improvements would include, but not be limited to, traffic control, water lines, sewer lines and storm drainage lines. To the extent that acquisition or subsequent CEQA clearance is necessary for such work, which would be the responsibility of the developer.

### **POLICY ANALYSIS**

The project site is designated in the General Plan as Medium Density Residential (MR). This designation provides for single-family detached and attached homes, public and quasi-public uses and similar and compatible uses. The project site is zoned Single Family Residential 6,000 Square Foot Average Minimum (R-2). Religious Institutions are a conditional use in the R-2 zone and are subject to obtaining a Use Permit from the Planning Commission per Chapter 17.52.020 of the Winters Municipal Code.

**Planned Development Overlay Zone and Planned Development Permit:** In March 2009, the Planning Commission granted a Planned Development Overlay Zone and Planned Development Permit that allows the church to exceed building height requirements and reduce the required on-site parking. In March 2010, both of these planning deviations were extended an additional two years, to April 2012. Since the application for the church redesign was submitted prior to the extension expiring, the Planning Commission's prior decision which allows the church to exceed the height requirement and permit a 10% reduction in the parking requirement remain intact and no legislative action is necessary. Staff confirmed that the proposed church redesign does not exceed the 52 feet in height permitted by the Planning Commission and a 10% parking reduction is not necessary because the church meets the City's parking requirement based on the downsizing of the church.

**Conditional Use Permit:** Public and quasi-public use such as churches are a conditional use in the R-2 zone. Although the site is currently developed with an actively used community center, the proposed 436 seat church represents a potentially significant intensification of use. The separation between public and quasi-public uses of the project and the adjoining residential neighborhood is a concern.

Currently the project site is separated from the backyards of the adjoining residential neighborhood by an unevenly maintained wood fence. The proposed intensification of the project site could subject the adjoining backyards and homes to additional loss of privacy and impacts from parking lot noise, headlights, and increased on-site activity. When the Planning Commission reviewed the project in March 2009, staff included a requirement that a 6-foot tall masonry block wall be installed along the south boundary of the project backing up to the residential housing. This requirement was discussed in detail at the Planning Commission meeting and the Commission directed staff to modify the project condition striking the

requirement the wall had to be masonry block but the construction of a wall needed to minimize noise and light impacts and provide privacy for the adjoining residences. Construction materials and design of the wall shall be subject to the review and approval of the Community Development Director. Staff has carried forward the Commission's condition from the March 2009 meeting into the Conditions of Approval and met with the applicant to discuss design options. After considering design solutions, the applicant requests the former condition be removed from the current planning entitlement request. The applicant has provided a justification to support their request (Applicant Narrative Response, Attachment F). After consideration of the applicant's request, staff has concluded adjacency issues such as additional loss of privacy and impacts from parking lot noise, headlights, and increased on-site can be minimized with a new screening wall. Staff is recommending the condition remain and construction materials and design of the wall shall be subject to further review and approval.

**Design Review:** The proposed church features Spanish Colonial influenced architecture which is consistent with the existing community center. The proposed church redesign would be constructed with exterior materials common to residential and public and quasi-public development in the area. Architectural materials include painted stucco, stained wood and red clay tile roofing. These design details are consistent with the previously approved design/site plan review made by the Planning Commission in March 2009 (Illustrative Elevations, Attachment G). The proposed redesigned church does include some subtle differences from the prior submission that enhance the overall design quality and layout. The following lists details these changes and the attached comparative site plan provides greater detail of the proposed changes (Comparative Site Plan and Elevation, Attachment H):

- Redesigned planters, raised them and added cap for seating
- Relocated and redesigned the water fountain
- Added bike racks
- Added planters on grade
- Downsized church and moved slightly south on site
- Larger landscape area
- Relocated trash enclosure
- Reworked the parking lot and added two spaces
- Added landscape to what was "future rectory"
- Meandering walk as requested by the City
- Arbor added between the parish center and church
- New bell stand
- Area need for compliance, landscape removed for walk path clearances

The proposed site design and on-site parking and circulation have been reviewed and found acceptable by the Fire District, and the City's Engineering, Building, and Planning Departments. The site's now mature landscaping soften the site and provide visual interest. The applicant has submitted preliminary landscape plans and lighting analysis. The existing landscaping along Grant Avenue and Main Street as well as the interior of the site will be retained and enhanced.

The potential for spill over of light and glare from the parking areas to the adjacent residential backyards and homes is a concern. Mitigation Measure Aesthetics 1 addresses this issue by requiring that outdoor light fixtures shall be low-intensity, shielded and/or directed away from adjacent areas and the night sky. All light fixtures shall be installed and shielded in such a manner that no light rays are emitted from the fixture at angles above the horizontal plane. Lighting plans with certification that adjacent areas will not be adversely affected and that offsite

illumination will not exceed 2-foot candles shall be submitted to the City for review and approval as part of improvement plans.

### **CEQA CLEARANCE**

A Mitigated Negative Declaration was previously prepared in accordance with the California Environmental Quality Act (CEQA) and certified by the Planning Commission on March 24, 2009 for a 13,813 square foot, 700 seat church and a future 2,900 square foot rectory with a two car garage (Initial Study, Attachment I). St. Anthony's church is proposing to reduce the size of the church with less seating thus reducing the overall impacts that were identified in the previous environmental document. In cases where a previous environmental document was prepared, the CEQA guidelines permit the lead agency to rely on the previous analysis.

Staff has determined the previously certified Mitigated Negative Declaration is the appropriate level of environmental review for the project because none of the items described in Section 15162 of the CEQA Guidelines warranting new environmental review of the project have occurred. Conditions that warrant additional environmental review include substantial changes to the project involving new significant environmental impacts or a substantial increase in the severity of previously identified significant effects; substantial changes in the circumstances under which the project is undertaken; or new information of substantial importance. The project as proposed is smaller in size and no new or increased effects from the project have been identified. In addition, the circumstances under which the project is undertaken have not substantially changed and no new substantial information has been received since the Mitigated Negative Declaration was adopted by the City.

### **PLANNING COMMISSION ACTION**

Subject to the attached conditions of approval, the staff recommends that the Planning Commission take the following action:

1. Certify that the Planning Commission has determined that the previously certified Mitigated Negative Declaration is the appropriate level of environmental review and that none of the conditions described in Section 15162 of the CEQA Guidelines warranting new environmental review exists.
2. Approve Conditional Use Permit to allow a religious institution in an R-2 zone.
3. Approve Design Review of site plan, building architectural design, architectural materials, architectural finishes, color schemes, and conceptual landscape plan for the church and associated site improvements.

### **ATTACHMENTS**

- A. Findings and Conditions of Approval
- B. Vicinity Map
- C. Site Plan
- D. Preliminary Floor Plan
- E. Building Elevations
- F. Applicant Narrative Response
- G. Illustrative Elevations
- H. Comparative Site Plan and Elevation
- I. Initial Study/Mitigated Negative Declaration



# ATTACHMENT "A"



# **FINDINGS OF FACT AND CONDITIONS OF APPROVAL FOR THE ST. ANTHONY'S CHURCH PROJECT**

## **FINDINGS OF FACT**

### **Findings for Reliance on Previously Certified Mitigated Negative Declaration**

1. The Planning Commission considered the Mitigated Negative Declaration before making a decision on the project on March 24, 2009.
2. On March 24, 2009, the Planning Commission considered comments received on the Mitigated Negative Declaration during the public review process.
3. On March 24, 2009, the Planning Commission found that the environmental checklist/initial study identified potentially significant effects, but: a) mitigation measures agreed to by the Applicant before the mitigated negative declaration and initial study were released for public review would avoid the effects or mitigate the effects to a point where clearly no significant impact would occur; and b) there is no substantial evidence, in light of the whole record before the City, that the project as revised to include the mitigation measures would have a significant effect on the environment.
4. The Certified Mitigated Negative Declaration reflects the independent judgment and analysis of the City of Winters.
5. The Certified Mitigated Negative Declaration has been prepared in compliance with CEQA and the State CEQA Guidelines, and as amended/revised is determined to be complete and final.
6. The custodian of the documents, and other materials, which constitute the record of proceedings is the Community Development Director. The location of these items is the office of the Community Development Department at City Hall, 318 First Street, Winters, California 95694.
7. The Mitigation Monitoring Plan was adopted on March 24, 2012 to ensure implementation of mitigation measures identified in the Mitigated Negative Declaration. The Planning Commission found that these mitigation measures are fully enforceable as conditions of approval of the project, and shall be binding on the Applicant, future property owners, and affected parties.
8. The City, as the lead agency determined the previously certified Mitigated Negative Declaration is the appropriate level of environmental review for the project because none of the items described in Section 15162 of the CEQA Guidelines warranting new environmental review of the project have occurred.

### **Findings for Conditional Use Permit for a Religious Institution in an R-2 Zone**

1. The property is designated Medium Density Residential. This designation provides for single-family detached and attached homes, public and quasi-public uses and similar and compatible uses. Religious institutions are considered a quasi-public use per Section 17.08.060.k of the Winters Municipal Code. Churches are a common component of such uses and, as conditioned, would be in conformity with the General Plan.
2. The project site is zoned Single Family Residential 6,000 Square Foot Average Minimum (R-2). Religious Institutions are a conditional use in the R-2 zone and are subject obtaining a Use Permit from the Planning Commission per Chapter 17.52.020 of the Winters Municipal Code.
3. Religious institutions including churches are consistent with the intent and purposes of the C-2 zone. The property is located on the Grant Avenue corridor adjacent to two other public and quasi-public uses and, as conditioned, will not detrimentally impact the character of the neighborhood.
4. The requested use, as conditioned, will not be detrimental to the public health, safety or general welfare.
5. As provided for via the conditions of approval, adequate utilities, access roads, drainage, sanitation and/or other necessary facilities or services will be provided.
6. The requested use, as conditioned, will not create a nuisance or enforcement problem within the neighborhood.
7. The requested use, as conditioned, will not result in a negative fiscal impact to the City.

### **Findings for Design Review**

1. The overall visible mass of the structures is appropriate and consistent with the existing architectural style for the site.
2. The proposed use and quality of the exterior construction material provides long-term compatibility with the general setting of the property and the visual character of the general neighborhood.
3. The architectural design of the buildings provides visual interest and varied detail to provide overall character and consistency with the existing development of the site.
4. Per site design, and as required by the conditions of approval, ground and roof mounted equipment is screened from public view.
5. Per the conceptual landscaping plan, and as required by the conditions of approval, the landscaping, site details, and amenities such as plazas, trellis and pedestrian areas are consistent with the Winters Municipal Code

### **CONDITIONS OF APPROVAL**

The following conditions of approval are required to be satisfied by the Applicant/Developer prior to Final Map, unless otherwise stated.

#### **General**

1. In the event any claim, action or proceeding is commenced naming the City or its agents, officers, and employees as defendant, respondent or cross defendant arising or alleged to arise from the City's approval of this project, the project Applicant shall defend, indemnify, and hold harmless the City or its agents, officers and employees, from liability, damages, penalties, costs or expense in any such claim, action, or proceeding to attach, set aside, void, or annul an approval of the City of Winters, the Winters Planning Commission, any advisory agency to the City and local district, or the Winters City Council. Project Applicant shall defend such action at Applicant's sole cost and expense which includes court costs and attorney fees. The City shall promptly notify the Applicant of any such claim, action, or proceeding and shall cooperate fully in the defense. Nothing in this condition shall be construed to prohibit the City of Winters from participating in the defense of any claim, action, or proceeding, if City bears its own attorney fees and cost, and defends the action in good faith. Applicant shall not be required to pay or perform any settlement unless the subdivider in good faith approves the settlement, and the settlement imposes no direct or indirect cost on the City of Winters, or its agents, officers, and employees, the Winters Planning commission, any advisory agency to the City, local district and the City Council.
2. The project is as described in the October 30, 2012 Planning Commission staff report. The project shall be constructed as depicted on the maps and exhibits included in the October 30, 2012 Planning Commission staff report, except as modified by these conditions of approval. Substantive modifications require public hearing(s) and Planning Commission action.
3. The approval of the use permit will expire on October 30, 2013 (one year) if the project has not begun operation. According to Winters Municipal Code Section 17.20.060 (Extension of time for use permits), the Community Development Director may approve a one-time extension of time for use permits. Such extension shall be approved for not more than one year.
4. Winters Municipal Code Section 17.36.060 (Revocation – Expiration – Modification) states that site plan approval for design review is subject to the same provisions that appear in Section 17.24.070 (A) (Variances - Revocation). In absence of regulations regarding extension of time for Design Review/Site Plan approval, a one-time extension of time of the Design Review/Site Plan approval is permitted for not more than two years from the date of project approval.
5. The applicant / owner shall pay all applicable City fees and charges at the rate and amount in effect at the time such fees and charges become due and payable.
6. The applicant shall comply with requirements of all other agencies of jurisdiction.

## Negative Declaration Mitigation Measures

7. **Mitigation Measure Aesthetics 1 – Outdoor light fixtures shall be low-intensity, shielded and/or directed away from adjacent areas and the night sky. All light fixtures shall be installed and shielded in such a manner that no light rays are emitted from the fixture at angles above the horizontal plane. Lighting plans with certification that adjacent areas will not be adversely affected and that offsite illumination will not exceed 2-foot candles shall be submitted to the City for review and approval as part of improvement plans.**

Prior to issuance of a building permit, the applicant shall submit a photometric and proposed lighting plan for the project to the satisfaction of the Community Development Department to ensure no spillover light and glare onto adjoining properties.

### 8. Mitigation Measure Air 1

- a. **Construction equipment exhaust emissions shall not exceed District Rule 2-11 Visible Emission limitations.**
- b. **Construction equipment shall minimize idling time to 5 minutes or less. Catalyst and filtration technologies shall be incorporated where feasible.**
- c. **The prime contractor shall submit to the District a comprehensive inventory (i.e. make, model, year, emission rating) of all the heavy-duty off-road equipment (50 horsepower or greater) that will be used an aggregate of 40 or more hours for the construction project. District personnel, with assistance from the California Air Resources Board, will conduct initial Visible Emission Evaluations of all heavy-duty equipment on the inventory list.**
  - i. **An enforcement plan shall be established to weekly evaluate project-related on-and-off-road heavy-duty vehicle engine emission opacities, using standards as defined in California Code of Regulations, Title 13, Sections 2180 - 2194. An Environmental Coordinator, CARB-certified to perform Visible Emissions Evaluations (VEE), shall routinely evaluate project related off-road and heavy duty on-road equipment emissions for compliance with this requirement. Operators of vehicles and equipment found to exceed opacity limits will be notified and the equipment must be repaired within 72 hours.**
  - ii. **Construction contracts shall stipulate that at least 20% of the heavy-duty off-road equipment included in the inventory be powered by CARB certified off-road engines, as follows:**

|                    |                        |
|--------------------|------------------------|
| 1. 175 hp - 750 hp | 1996 and newer engines |
| 2. 100 hp - 174 hp | 1997 and newer engines |
| 3. 50 hp- 99 hp    | 1998 and newer engines |
  - iii. **In lieu of or in addition to this requirement, the applicant may use other measures to reduce particulate matter and nitrogen oxide emissions from project construction through the use of emulsified diesel fuel and or particulate matter traps. These alternative measures, if proposed, shall be developed in consultation with District staff.**

### 9. Mitigation Measure Air 2

- a. **Nontoxic soil stabilizers according to manufacturer's specifications shall be applied to all inactive construction areas (previously graded areas inactive for ten days or more).**
- b. **Ground cover shall be reestablished in disturbed areas quickly.**

- c. Active construction sites shall be watered at least three times daily to avoid visible dust plumes.
  - d. Paving, applying water three times daily, or applying (non-toxic) soil stabilizers shall occur on all unpaved access roads, parking areas and staging areas at construction sites
  - e. Enclosing, covering, watering daily, or applying non-toxic soil binders to exposed stockpiles (dirt, sand, etc.) shall occur.
  - f. A speed limit of 15 MPH for equipment and vehicles operated on unpaved areas shall be enforced.
  - g. All vehicles hauling dirt, sand, soil, or other loose materials shall be covered or shall be maintained at least two feet of freeboard.
  - h. Streets shall be swept at the end of the day if visible soil material is carried onto adjacent public paved roads.
10. **Mitigation Measure Biological 1** – The project proponent shall mitigate for potential project-related impacts to burrowing owl by conducting a pre-construction survey no more than 30 days prior to the initiation of construction activity. The pre-construction survey shall be conducted by a qualified biologist familiar with the identification of burrowing owls and the signs of burrowing owl activity. If active burrows are found on the project site, the California Department of Fish and Game (CDFG) shall be consulted regarding appropriate mitigation measures for project-related impacts to burrowing owl. Pursuant to the CDFG document entitled "Staff Report on Burrowing Owl Mitigation" (September 25, 1995), it is likely that replacement habitat will be required by CDFG. The guidelines include specific mitigation to protect nesting and wintering owls and to compensate for loss of breeding sites. In general, if the project would remove habitat of an occupied breeding site (e.g., if an active nest and surrounding habitat are removed), the project proponent will be required to compensate by preserving equivalent suitable habitat for each active nest site. In addition, the project proponent must install artificial burrows to offset the direct loss of the breeding site. Mitigation shall be consistent with the City's adopted Habitat Mitigation Program. Implementation of this mitigation measure shall be confirmed by the City of Winters prior to the initiation of construction activity.
11. **Mitigation Measure Biological 2** – The project proponent shall mitigate for potential project-related impacts to nesting raptors (Swainson's Hawk, White-tailed Kite, Northern Harrier, and Loggerhead Shrike) by conducting a pre-construction survey of all trees suitable for use by nesting raptors on the subject property or within 0.25 mile of the project boundary as allowable. The preconstruction survey shall be performed no more than 30 days prior to the implementation of construction activities. The preconstruction survey shall be conducted by a qualified biologist familiar with the identification of raptors known to occur in the vicinity of the City of Winters. If active special-status raptor nests are found during the preconstruction survey, a 0.25-mile (1,320-foot) buffer zone shall be established around the nest and no construction activity shall be conducted within this zone during the raptor nesting season. The buffer zone shall be marked with flagging, construction lathe, or other means to mark the boundary of the buffer zone. All construction personnel shall be notified as to the existence of the buffer zone and to avoid entering the buffer zone during the nesting season. Implementation of this mitigation measure shall be confirmed by the City of Winters prior to the initiation of construction activity.
12. **Mitigation Measure Biological 3** – The project proponent shall mitigate for potential project-related impacts to migratory birds by conducting a pre-construction survey for nests on the site. The preconstruction survey shall be performed no more than 14 days prior to the onset of vegetation and/or tree removal. The preconstruction survey shall be conducted by a qualified biologist familiar with the identification of migratory bird known to occur in the vicinity of the City of Winters. If active migratory bird nest(s) are found onsite during the preconstruction survey, the nest(s) shall not be disturbed or removed until the young have fledged and the nest is no longer active. A buffer may be required. All construction personnel shall be notified as to the existence of the buffer zone and to

avoid entering the buffer zone during the nesting season. Implementation of this mitigation measure shall be confirmed by the City of Winters prior to the initiation of construction activity.

Alternatively, potential impacts to nesting birds or unfledged young would be avoided if vegetation and/or tree removal occurred only between September 1 and January 21.

13. Mitigation Measure Biological 4 – Any mitigation required shall be implemented in a manner consistent with requirements, purpose and intent of the City of Winters' Habitat Mitigation Program.
14. Mitigation Measure Cultural 1 – If cultural resources (historic, archeological, paleontological, and/or human remains) are encountered during construction, workers shall not alter the materials or their context until an appropriately trained cultural resource consultant has evaluated the situation. Project personnel shall not collect cultural resources. Prehistoric resources include chert or obsidian flakes, projectile points, mortars, pestles, dark friable soil containing shell and bone dietary debris, heat-affected rock, or human burials. Historic resources include stone or adobe foundations or walls, structures and remains with square nails, and refuse deposits often in old wells and privies.
15. Mitigation Measure Cultural 2 - Should human remains be discovered, no further site disturbance shall occur until the county coroner has determined that the remains are not subject to the provisions of Section 27491 of the Government Code or any other related provisions of law concerning investigation of the circumstances, manner and cause of any death, and the recommendations concerning the treatment and disposition of the human remains have been made to the person responsible for the excavation, in the manner provided in Section 5097.98 of the Public Resources Code. If the coroner determines that the remains are not subject to his or her authority and the remains are recognized to be those of a Native American, the coroner shall contact the Native American Heritage Commission within 24 hours.
16. Mitigation Measure Geology 1 – The applicant shall submit a soils and geotechnical report upon submittal of the initial improvement plans package. The improvement plans shall be signed by the soils engineer for conformance to the geotechnical report prior to approval by the City.
17. Mitigation Measure Land Use 1 – All aspects of the project shall be subject to design review to ensure compatibility with the surrounding area and satisfaction of the Community Design Guidelines and other applicable principles of good community design.
18. Mitigation Measure Land Use 2 – The proposed project height and parking provisions are subject to approval of a Planned Development Overlay for the subject property.
19. Mitigation Measures Noise 1- The project applicant shall submit a construction noise mitigation plan to the City of Winters for review and approval. The plan shall depict the location of construction equipment and describe how noise would be mitigated through methods such as, but not limited to, locating stationary noise-generating equipment (such as pumps and generators) as far as possible from nearby noise-sensitive receptors. Where practicable, noise-generating equipment will be shielded from nearby noise-sensitive receptors by noise-attenuating buffers such as structures or haul trucks. Onsite noise sources such as heavy equipment located less than 200 feet from noise-sensitive receptors will be equipped with noise-reducing engine housings. Portable acoustic barriers able to attenuate at least 6 dB will be placed around noise-generating equipment located within 200 feet of existing residences. Water tanks and equipment storage, staging, and warm-up areas shall be located as far from noise-sensitive receptors as possible. All noise attenuation measures identified in the plan shall be incorporated into the project.
20. Mitigation Measure Noise 2 - Construction activities shall adhere to the following noise requirements:

All construction equipment shall utilize noise reduction features (e.g., mufflers and engine shrouds) that are no less effective than those originally installed by the manufacturer.

Hours of construction shall comply with those established in Chapter 8.20.100 of the Winters

Municipal Code. Those hours are weekdays from 7:00 a.m. through 7:00 p.m. Construction is prohibited on weekends and federal holidays.

21. **Mitigation Measure Utilities 1** – The proposed systems for conveying project sewage, water, and drainage shall be finalized and approved by the City Engineer prior to approval of improvement plans. The project is required to fund and construct off-site improvements necessary to support the development. Such improvements could include, but not be limited to a water well, water lines, sewer lines and storm drainage lines. Should property acquisition or additional CEQA clearance be required for off-site improvements, this will be the responsibility of the developer.
22. **Mitigation Measure Utilities 2** – A Certificate of Occupancy shall be issued only after the City Engineer has established that water supply will be available to serve the building.

#### **Conditional Use Permit for Religious Institution in a R-2 Zone**

23. The applicant shall submit a landscape, irrigation, lighting, and fencing plan to City for review and approval prior to approval of the improvement plans.
24. The applicant shall install 6-foot tall wall along the south boundary of the project that backs up to the residential housing to minimize noise and light impacts and provide privacy for the adjoining residences. Construction materials and design of the wall shall be subject to the review and approval of the Community Development Director.

#### **Design Review Conditions**

23. Building design and theme shall be the Spanish Colonial style as approved by the Planning Commission on October 30, 2012.
24. Light fixtures attached to buildings shall be designed as an integral part of the building facades to highlight building forms and architectural details.
25. Lighting plan shall be subject to approval by the Community Development Director.
26. Exterior building colors and materials shall be consistent with the color schemes and materials approved by Planning Commission on October 30, 2012.
27. Landscaping and signage shall be consistent with the applicable requirements of Chapters 17.76 (Landscaping and Design) and 17.80 (Signs) of the Winters Municipal Code. Signage and landscaping shall be subject to approval by the Community Development Director.
28. Prior to the issuance of a building permit, the Community Development Director and Public Works Director shall review and approve the design and siting of trash facilities. Trash and recycling facilities shall be enclosed. The trash and solid waste facilities shall incorporate design features for the project that are conducive to collecting and storing recyclables and shall incorporate recycling collection at a designated facility within the site area at appropriate locations.
29. Prior to issuance of a certificate of occupancy, the project landscape architect shall conform to the Community Development Director that all on-site landscaping is completed and in accordance with the final building permit and improvement plans, including off-site and public improvements, or that other acceptable arrangements acceptable have been made for ensuring that the work is completed, such as an irrevocable standby letter of credit to cover all costs of the unfinished work plus 25 percent.
30. Prior to issuance of a certificate of occupancy, the project landscape architect shall confirm to the Public Works Director that all off-site landscaping is completed and in accordance with the final building permit and improvement plans, including off-site and public improvements, or that other

acceptable arrangements acceptable have been made for ensuring that the work is completed, such as an irrevocable standby letter of credit to cover all costs of the unfinished work plus 25 percent.

31. Prior to issuance of a certificate of occupancy, the Community Development Director shall confirm that all mechanical equipment, including electrical and gas meters, heating/air conditioning or ventilation units, radio/TV antennas or satellite dishes shall be appropriately screened from off-site view, and electrical transformers shall be either placed underground or appropriately screened.

#### **Community Development Conditions**

32. Foundations shall be poured in place, onsite. No pre-cast foundations will be permitted. This shall be stipulated in all construction contracts.
33. All address numbering shall be clearly visible from the street fronting the property. All buildings shall be identified by either four (4) inch illuminated numbers or six (6) inch non-illuminated numbers on contrasting background. Address for each unit shall be clearly visible for each unit and shall be architecturally consistent with building design.
34. The Applicant shall pay all development impact fees, fees required by other entities, and permit fees.
35. The Applicant shall be responsible for any additional costs associated with the processing of this project including but not limited to: plan check, inspections, materials testing, construction monitoring, and other staff review and/or oversight including staff time necessary to ensure completion/satisfaction of all conditions of approval and mitigation measures. The Applicant shall, on a monthly basis, reimburse the City for all such costs. Project Applicant shall pay all development impact fees adopted by the City Council and shall pay fees required by other entities.
36. The main electrical panel for each building shall be located at the exterior of the building and capable of total electrical disconnect by a single throw. This same requirement shall apply to each sub-unit or office suite unless waived by the Fire and Community Development Departments.
37. Each building shall be wired for security and fire alarm systems.
38. Buildings shall be wired to enable WiFi security monitoring of project site.
39. There shall be no outside storage of any type in parking areas. Those areas shall be kept free of obstruction and available for their designated use.
40. The site and improvements shall be well maintained and kept free of litter, debris, weeds and graffiti. Any graffiti shall be removed within 72 hours of discovery in a manner which retains the existing color and texture of the original wall or fence as most practically feasible.
41. The project shall operate in a manner to limit noise exposure to those levels set forth in the Winters Municipal Code and General Plan.
42. Bike racks shall be provided per Winters Municipal Code and be located adjacent to each building. Locations shall be approved by the Community Development Department.

#### **Public Works Department/City Engineering Conditions**

43. Project applicant shall pay all development impact fees adopted by the City Council at the rate in effect at the time of building permit issuance and shall pay fees required by other entities.
44. The applicant shall satisfy all agencies of jurisdiction and satisfy all City of Winters requirements for development.

45. A Public Improvement Agreement shall be entered into and recorded prior to construction of improvements and/or issuance of any building permits.
46. Proposed improvements, including but not limited to, grading, streets, utilities, and landscape have not been reviewed in detail and are not approved at this time. The City Engineer shall review the design of all improvements, during the plan check process and shall be revised, as needed, at the discretion of the City Engineer.
47. The applicant shall, on a monthly basis, reimburse the City for all costs which are not otherwise provided for in the approval of this project including permit fees, inspections for work in public right-of-way, materials testing, construction monitoring, plan checks and reviews, and other hard costs incurred by the project.
48. Grant Avenue (SR128) - The Applicant shall construct/widen Grant Avenue. Improvements shall include but not be limited to frontage improvements to include street pavement widening, landscaping, 10 foot pedestrian/Bike path, driveway, and under-grounding overhead utilities. The applicant shall be responsible for all costs associated with Grant Avenue frontage improvements. For work within the State Highway right of way, the Applicant shall submit an Encroachment Permit Application for State approval of proposed improvements.
49. West Main Street- The Applicant shall construct street frontage improvements to include landscaping, 8 foot sidewalk, driveway, and under grounding overhead utilities. The applicant shall be responsible for all costs associated with West Main Street frontage improvements.
50. A signage and striping plan is required and shall be approved by the City Engineer. All striping shall be thermoplastic.
51. The applicant shall contact the City Engineer prior to beginning construction for a pre-construction meeting.
52. The applicant shall install one or more fire hydrants pursuant to City of Winters Public Works Department Improvement Standards. The number and location of the fire hydrants shall be determined by the Fire Chief. The installation of the fire hydrants shall comply with the specifications of the City of Winters Public Works Improvement Standards and Construction Specifications. Prior to hydrant approval, the water system shall be flushed to remove foreign matter in the system. All unfinished installation water mains or their appendages or openings shall be covered in such a manner that foreign matter does not enter the water system.
53. A hydrant use permit shall be obtained from the Public Works Department, for water used in the course of construction.
54. The City Engineer and Fire Chief shall review and approve the location, number, and specifications of the backflow devices.
55. Water meters shall be installed on all water services to the satisfaction of the City Engineer.
56. The applicant shall submit to the City Engineer for review and approval a storm drainage plan for the project area, prior to the approval of the improvement plans. The applicant shall be responsible for acquisition of all storm drain or other easements from adjacent property owners, if applicable, which are required for the construction and maintenance of perimeter and off-site improvements.
57. All perimeter parcels and lots shall be protected against surface runoff from adjacent properties in a manner acceptable to the City Engineer.
58. At the time of making the survey for the development, the engineer or surveyor shall set sufficient durable monuments to conform to the standards described in Section 8771 of the Business and

Professions Code. All monuments necessary to establish the exterior boundaries of the project shall be set or referenced prior to final acceptance of project.

59. Grading shall be done in accordance with a grading plan prepared by the applicant's civil engineer and approved by the City Engineer. The amount of earth removed shall not exceed that specified in the approved grading plan. All grading work shall be performed in one continuous operation. The grading plans shall be included in the improvement plans. In addition to grading information, the grading plan shall indicate all existing trees and trees to be removed as a result of the proposed development, if any.
60. If disposal and sharing of the excavated soil from the construction of the Development occurs, prior to grading, Applicant shall prepare a written agreement with the other participating property owners and submit to the City.
61. The development shall include implementation of post-construction best management practices (BMP). Post construction BMP's shall be identified on improvement plans and approved by the City Engineer.
62. Construction of the project disturbing more than one acre of soil shall require a National Pollution Discharge Elimination System (NPDES) construction permit.
63. Construction of the project disturbing less than one acre of soil shall implement BMP's to prevent and minimize erosion. The improvement plans for construction of less than 1 acre shall include a BMP to be approved by the City Engineer.
64. An erosion and sedimentation control plan shall be included as part of the improvement plan package. The plan shall be prepared by the applicant's civil engineer and approved by the City Engineer. The plan shall include but not be limited to interim protection measure such as benching, sedimentation basins, energy dissipation structures, and check dams. The erosion control plan shall also include all necessary permanent erosion control measures, and shall include scheduling of work to coordinate closely with grading operations. Replanting of graded areas and cut and fill slopes is required and shall be indicated accordingly on plans, for approval by the City Engineer.
65. Applicants for projects draining into water bodies shall obtain a National Pollutant Discharge Elimination System (NPDES) Permit from the Regional Water Quality Control Board prior to commencement of grading.
66. All utilities within 100 feet of the project boundary shall be installed underground per the Ordinance No. 95-03, "An Ordinance Amending Article 6, Chapter 3, Title VII, Underground Utility Lines, of the Winters Municipal Code", and shall meet the policies, ordinances, and programs of the City of Winters and the utility providers. All utility services extended into the project site shall be underground.
67. On site utilities shall be privately owned.
68. Final Joint Trench utility plans shall be included with the improvement plans, prior to approval by the City Engineer.
69. Existing public and private facilities damaged during the course of construction shall be repaired by the applicant, at the applicant's sole expense, to the satisfaction of the City Engineer.
70. Occupancy of the proposed church shall not occur until off-site improvements (water, sewer, streets, etc.) have been constructed and approved by the City Engineer, and the City has approved as-built drawings, and the unit has been issued a Certificate of Occupancy by the Building Official.
71. Appropriate easements and rights of way shall be required for City maintained facilities located outside of City-owned property or the public right-of-way. The applicant shall facilitate, with City

cooperation, the abandonment of all City easements and dedications currently held but no longer necessary as determined by the Public Works Department.

72. All work within public right-of-way or easement shall comply with the City of Winters Public Works Improvement Standards and Construction Specifications, subject to the approval of the City Engineer.
73. The applicant shall provide a 10-foot public utility easement (PUE) along the frontage of the parcels.
74. The applicant/property owner shall agree to grant all public easements as determined by the City for public purposes.
75. The applicant shall abandon any well, septic tank, and leach field located on the property. The applicant shall provide a letter from the Yolo County Environmental Health Department giving location and filling specifications for all water wells or septic tanks within the project boundaries. If there are no wells or septic tanks, the applicant shall provide a letter so station from the Yolo County Environmental Health Department.
76. The owner of the property shall annex into the City-Wide Maintenance Assessment District in order to maintain and provide for the future needs of parks, open spaces, street lighting, landscaping and other related aspects and impacts from new development. The applicant shall fulfill this condition prior to or concurrent with the approval of the improvement plans.
77. The applicant shall submit a landscape, irrigation, lighting, and fencing, plan to City for review and approval prior to approval of the improvement plans. The applicant shall install a 6 foot tall masonry block wall along the south boundary of the project that backs up to the residential housing.
78. All public landscape areas shall include water laterals with meters and PG&E power service points for automatic controllers.
79. Developer shall pay appropriate reimbursements for benefiting improvements installed by others, in the amount and at the time specified by existing reimbursement agreements.
80. The applicant shall submit a soils and geotechnical report upon submittal of the initial improvement plans package. The improvement plans shall be signed by the soils engineer for conformance to the geotechnical report prior to approval by the City.
81. The applicant shall minimize the dust generated by construction of the project. Dust generated from construction shall not exceed standards established by the Yolo-Solano Air Quality Management District and the Community Development Department.
82. Tarpaulins or other effective covers should be used for haul trucks.
83. All inactive portions of the construction site, which have been graded will be seeded and watered until vegetation is grown.
84. Grading shall not occur when wind speeds exceeds 15 MPH over a one hour period.
85. Construction vehicle speed on unpaved roads shall not exceed 15 MPH.
86. Construction equipment and engines shall be properly maintained.
87. If air quality standards are exceeded in May through October, the construction schedule will be arranged to minimize the number of vehicles and equipment operating at the same time.
88. Construction practices should be augmented to minimize vehicle idling.
89. Potentially windblown materials will be watered or covered.

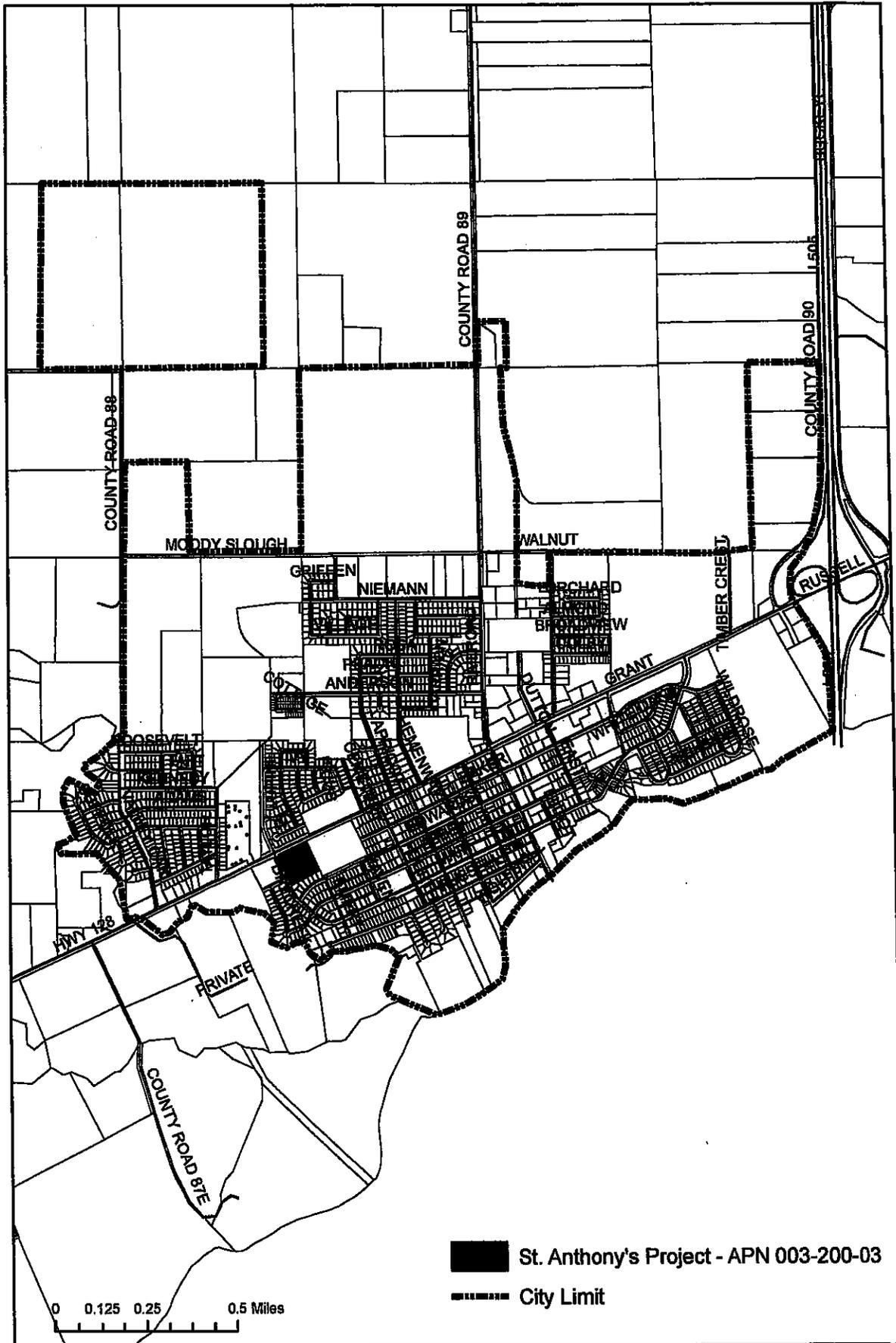
90. Construction areas and streets will be wet swept on a daily basis.
91. Applicant shall provide refuse enclosure detail showing bin locations, pad detail, and recycling facilities to the approval of the Public Works Department.
92. U.S. Post Office mailbox location shall be approved by the Winters Postmaster and shown on the improvement plans submitted to the City Engineer.
93. Per City of Winters Cross Connection Control Program, all types of commercial buildings and landscape irrigation services are required to maintain an approved backflow prevention assembly, at the applicant's expense. Service size and flow-rate for the backflow prevention assembly must be submitted. Location of the backflow prevention assembly shall be per the City of Winters Public Improvements Standards and Construction Standards. Prior to the installation of any backflow prevention assembly between the public water system and the owner's facility, the owner or contractor shall make application and receive approval from the City Engineer or his designated agent.
94. Landscaping and irrigation plans shall be prepared by a registered landscape architect, and included as part of the improvement plans and/or site plans. These plans shall be per City Standards and the Water Conservation in Landscaping Act of 2006 (AB 1881) and shall be subject to review and approval by the City. The improvement plans shall include landscaping and automatic irrigation for the public right-of-way of SR 128 and CR 90. Drought tolerant native plant species shall be incorporated into landscaping plans to the maximum extent possible and drip irrigation systems shall be used in the landscaping of new public and private open space areas. No substantial change to an approved landscaping or irrigation plan may be made without written approval by the original approving person or body.
95. All conditions identified herein shall be fully satisfied prior to occupancy, unless otherwise stated.
96. Occupancy shall not occur until on-site and off-site improvements have been accepted by the City Council and the City has approved as-built drawings.



# **ATTACHMENT “B”**



# St Anthony's Project - APN 003-200-03





# ATTACHMENT "C"

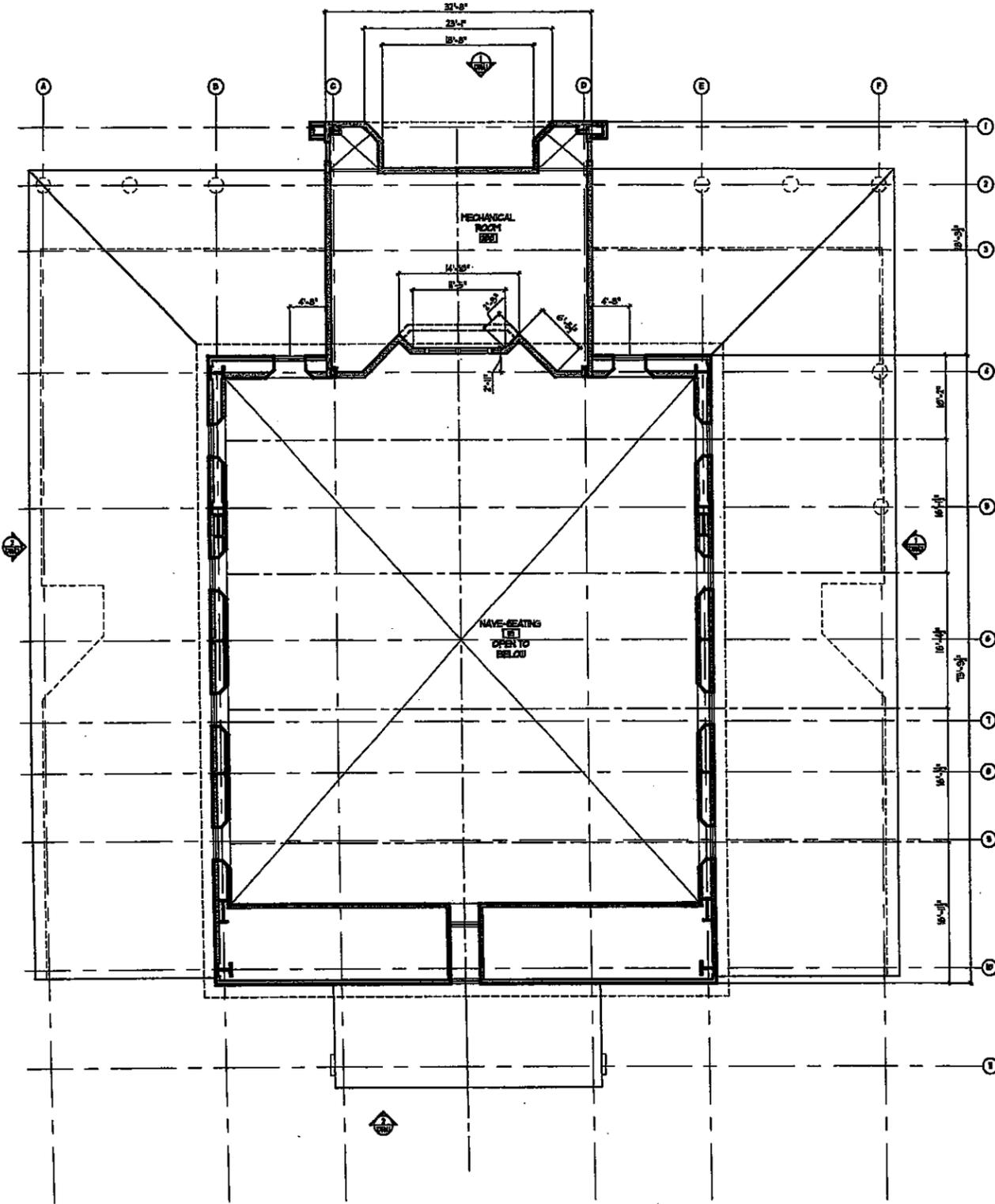




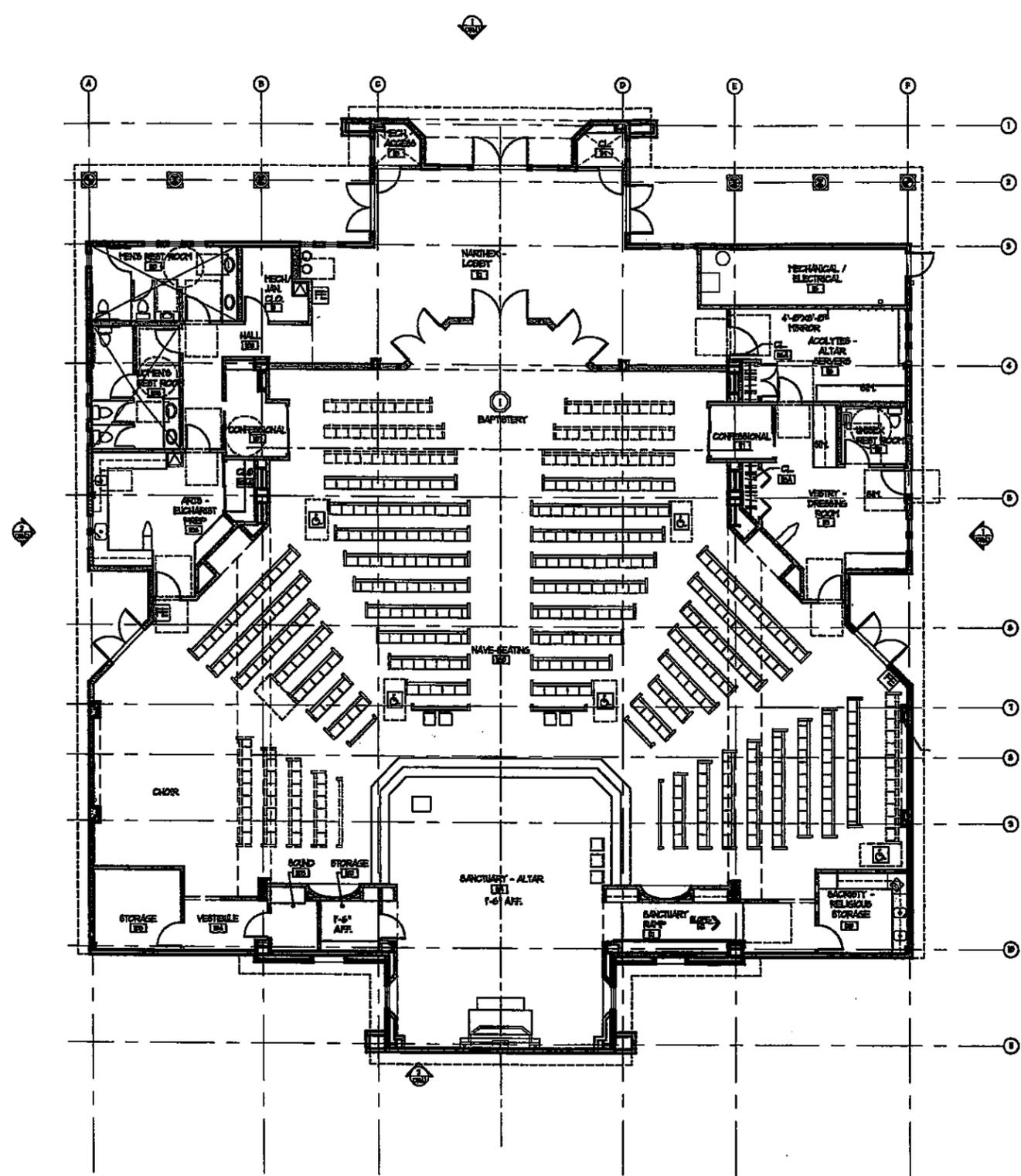


# ATTACHMENT "D"





2 MECHANICAL MEZZANINE PLAN SCALE: 1/8" = 1'-0"



1 GROUND FLOOR PLAN SCALE: 1/8" = 1'-0"

- LEGEND:**
- ROOM NUMBER [1]
  - GRID LINE [2]
  - FIRE EXTINGUISHER [3]
  - EXTERIOR ELEVATION [4]
  - INTERIOR WALL [5]
  - EXTERIOR WALL [6]
  - 1-HOUR RATED WALL [7]

- KEY NOTES:**
- [1] RECESSED NICHE W/ EXTERIOR FINISH - SLOPED TO DRAIN
  - [2] SLOPED INTERIOR GYP ED. GILL
  - [3] RINGS AS REQUIRED TO CONCEAL STRUCTURAL FRAME
  - [4] ALIGN W/ WALL AT FIRST FLOOR BELOW
  - [5] LOW ROOF AT FIRST FLOOR BELOW
  - [6] EDGE OF ROOF OVERHANGS ABOVE
  - [7] FUTURE SEATING

REVISIONS

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PROPOSED PROJECT:  
**ST. ANTHONY**  
 811 MAIN STREET  
 WINTERS, CALIFORNIA 95694

**McANDREWS & ASSOCIATES ARCHITECTS, INC.**  
 11400 14TH AVE. #204  
 SAN DIEGO, CA 92161  
 PHONE: (619) 591-0000  
 FAX: (619) 591-0008



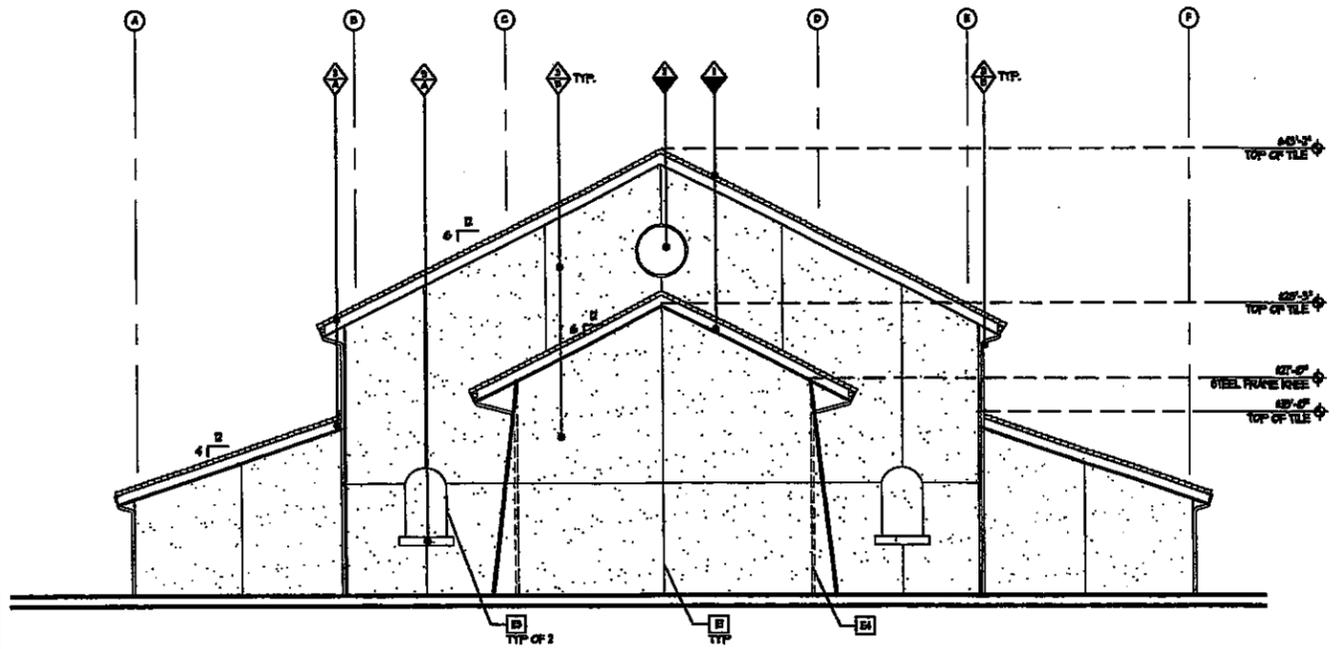
DESIGN REVIEW SET

DRAWN: MAA  
 CHECKED: MAA  
 DATE: 07-03-2012  
 SCALE: AS NOTED  
 JOB #: 12-165  
 SHEET: DR2  
 OF X SHEETS



# ATTACHMENT "E"





2 SOUTH ELEVATION

SCALE: 1/8" = 1'-0"

| MATERIAL |  |
|----------|--|
| 1        | CAYNE REAL 8123456 SANTA CRUZ SPAN & TILE BY BAGLE ROOFING PRODUCTS TO MATCH (B) MULT-PURPOSE BUILDING |
| 2        | FASCIA / GUTTER  |
| 3        | STUCCO   |
| 4        | METAL SIDING   |
| 5        | COLUMN COVER   |
| 6        | STAINED WOOD DOOR  |
| 7        | ALUMINUM TRILLS BY BAC AWINGS W/ DCSBPT FINISH   |
| 8        | UPRIGHT IRON CROSS   |
| 9        | FOAMSTUCCO   |
| 10       | STATUARY   |
| 11       | GLAZING  |
| 12       | METAL DOWNPOUT   |
| 13       | METAL DOOR   |

| FINISH |  |
|--------|--|
| A      | DANN EDWARDS 'SADDLE BROOD' DE 2164  |
| B      | DANN EDWARDS 'CHAFFANVAL' DE 145   |
| C      | CHESTNUT BRONZE  |
| D      | BLACK  |
| E      | SHURA SERIES COMPOSITE FIBERGLASS COLUMN COVER IN WHITE BY PACIFIC COLUMN OR EQUAL |
| F      | CLEAR  |
| G      | CRAFTERMAN WOOD VISE: 8TH DURANGO FINISH   |

**GENERAL ELEVATION NOTES**

- BUILDING STUCCO TO BE PAINTED B
- ALL EXTERIOR METAL DOORS TO BE PAINTED A
- COLUMNS TO BE PAINTED B.

**KEY NOTES**

- [B] STUCCO EXPANSION JOINT
- [D] RAISED, INTERNALLY ELIMINATED ADDRESS NUMBERS 1/2" MIN. 6" HIGH CHARACTERS ON CONTRASTING BACKGROUND
- [E] DECORATIVE RECESSED NOSE
- [F] LINE OF WALL BEYOND

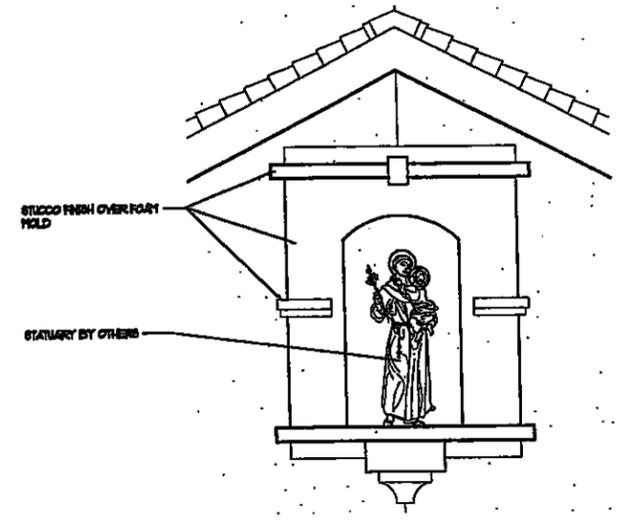
PROPOSED PROJECT:  
**ST. ANTHONY**  
 811 MAIN STREET  
 WINTERS, CALIFORNIA 95694

**MACCANDLER & ASSOCIATES ARCHITECTS, INC.**  
 200 W. MAIN STREET, SUITE 200  
 WINTERS, CALIFORNIA 95694  
 PHONE (209) 832-0000  
 FAX (209) 832-3300



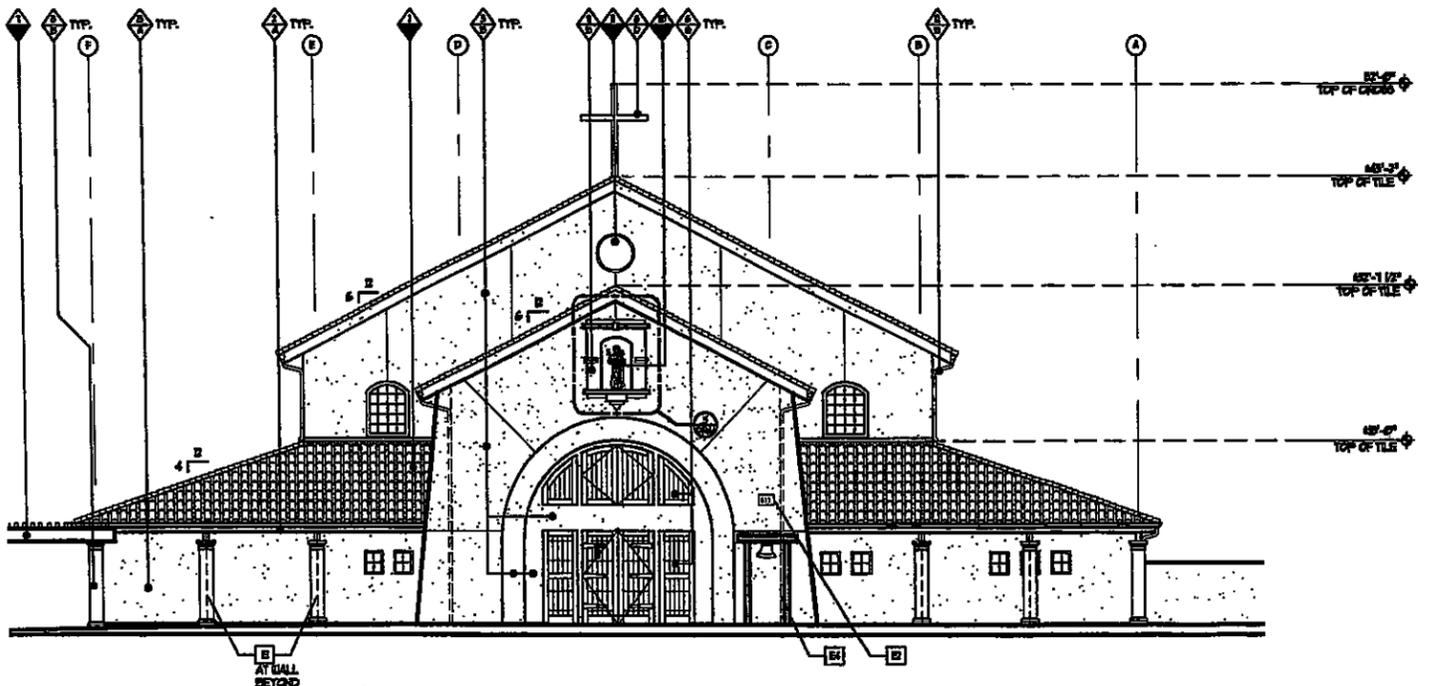
DESIGN REVIEW SET

DRAWN BY: [ ]  
 CHECKED BY: [ ]  
 DATE: 01-23-2012  
 SCALE:  
 JOB #:  
 SHEET #:  
**DR4.1**  
 OF X SHEETS



3 NICHE DETAIL @ MAIN ENTRY

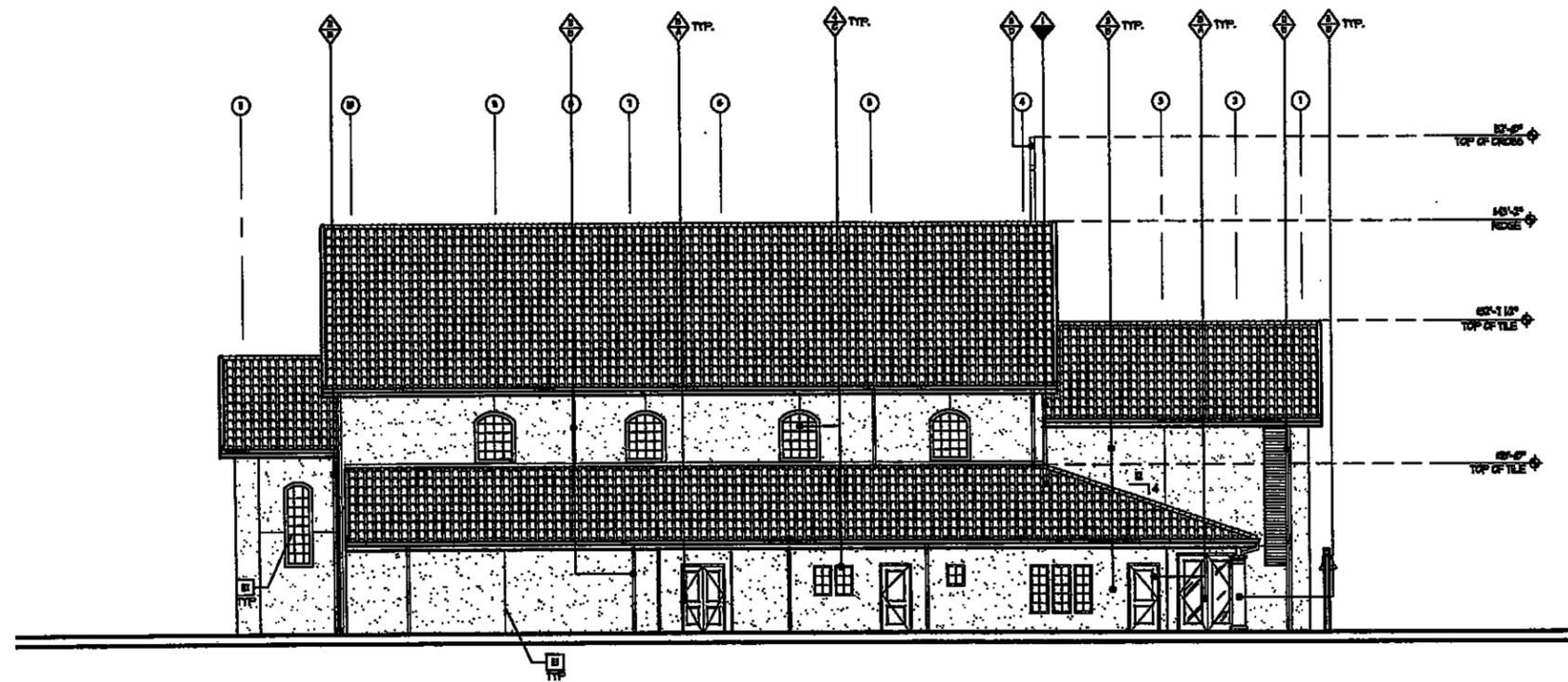
SCALE: 1/2" = 1'-0"



1 NORTH ELEVATION

SCALE: 1/8" = 1'-0"





1 EAST ELEVATION

SCALE: 1/8" = 1'-0"

| MATERIAL |   |
|----------|---|
| 1        | CAYNE REAL (UPGRADE SANTA CRUZ SPANISH & TILE BY EAGLE ROOFING PRODUCTS TO MATCH (B) MULTI-PURPOSE BUILDING |
| 2        | PASCA / GUTTER  |
| 3        | STUCCO  |
| 4        | METAL UNDOOR  |
| 5        | COLUMN COVER  |
| 6        | STAINED WOOD DOOR   |
| 7        | ALUMINUM TRELLIS BY DAC ARIZONA BY SCRIPT FINISH  |
| 8        | BROUGHT IRON CROSS  |
| 9        | FOAMSTUCCO  |
| 10       | STATUARY  |
| 11       | GLASS   |
| 12       | METAL DOWNPOUT  |
| 13       | METAL DOOR  |

| FINISH |  |
|--------|--|
| A      | DON EDUARDO 'MIDDLE BRUSH' DE 164  |
| B      | DON EDUARDO 'CHAFFARRAL' DE 146  |
| C      | CHESTNUT BRONZE  |
| D      | BLACK  |
| E      | EXONA SERIES COMPOSITE FIBERGLASS COLUMN COVER IN WHITE BY PACIFIC COLUMN OR EQUAL |
| F      | CLEAR  |
| G      | CRAFTSMAN IN WOOD VGR WITH DUNINGO FINISH  |

**GENERAL ELEVATION NOTES**

- BUILDING STUCCO TO BE PAINTED B
- ALL EXTERIOR METAL DOORS TO BE PAINTED A
- COLUMNS TO BE PAINTED B.

**KEY NOTES**

- [B] STUCCO EXPANDED JOINT
- [D] -
- [E] DECORATIVE RECESSED NICHE
- [H] LINE OF WALL BEYOND
- [I] OUTLINE OF MECHANICAL SCREEN WALL

| REVISIONS |
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PROPOSED PROJECT:  
**ST. ANTHONY**  
 811 MAIN STREET  
 UNTERS, CALIFORNIA 92694

**MACANILLOS & ASSOCIATES ARCHITECTS, INC.**  
 1111 17TH AVENUE, SUITE 200  
 COSTA MESA, CALIFORNIA 92626  
 PHONE (714) 441-1111  
 FAX (714) 441-1112

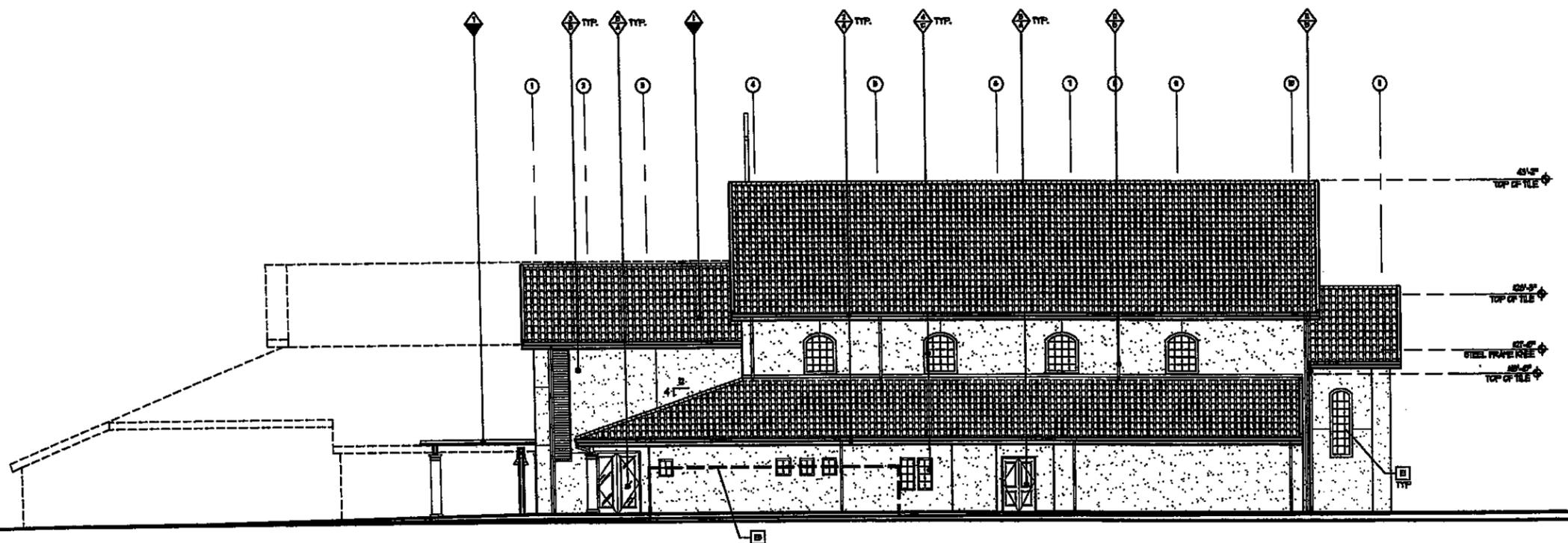


DESIGN REVIEW SET

DRAWN: MAA  
 CHECKED: MAA  
 DATE: 07-23-2012  
 SCALE:  
 JOB: 12-165  
 SHEET: DR42  
 OF X SHEETS

2 WEST ELEVATION

SCALE: 1/8" = 1'-0"





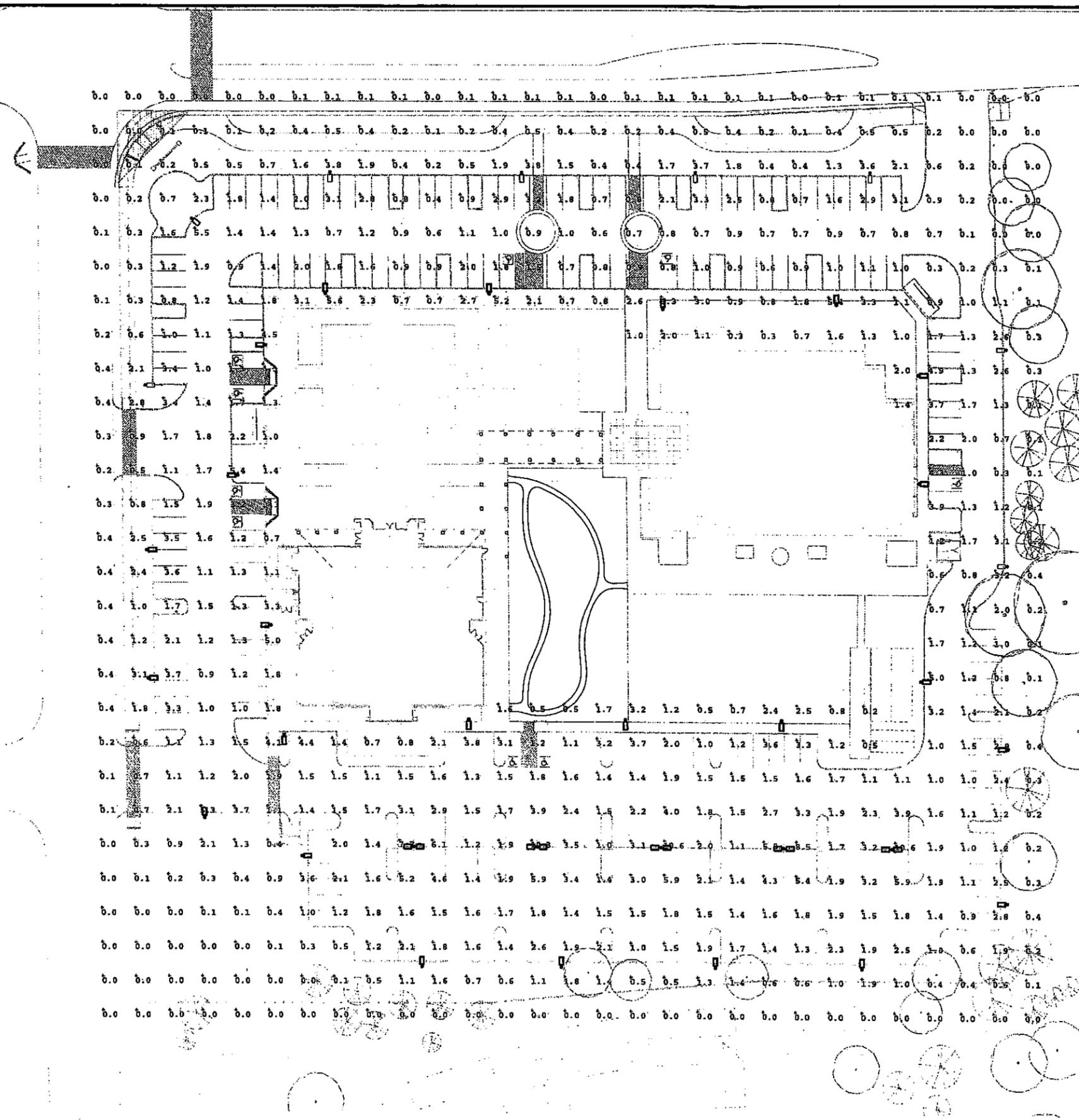
# ATTACHMENT "F"



**NARRATIVE RESPONSE TO THE  
CONDITIONS OF APPROVAL**

On behalf of the Owners, we would like to request that the former condition of approval to provide a continuous wall along the South property line be removed from any conditions of approval in the current Planning Entitlements for St. Anthony Parish Church. The conditions of approval requiring the wall was originally intended to mitigate light and sound transference between the project site and the 7 houses whose back yards abut the Southern property line of the project. Included in the construction documents and also attached herewith is sheet E2.2 which documents light levels for the entire site. It can be seen that the light levels at the residential locations are 0.0. Also included in the construction documents and attached herewith is sheet L5 showing the landscape design along the South property line. There are 25 trees of 8 varieties including redwood, cedar, camphor, walnut and four varieties of oak to be planted along the South property line. There is also a planting bed that varies in depth from the parking lot to the existing fence from 10 feet to 42 feet that will be fully planted full with a variety of shrubs. It is our contention that the intense planting along with the existing fence will be sufficient to mitigate the concerns that motivated the former conditions of approval.





SITE PLAN - PHOTOMETRICS

SCALE: 1" = 50'-0"



| REVISIONS |  |
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PROPOSED PROJECT:  
**ST. ANTHONY**  
 811 MAIN STREET  
 WINTERS, CALIFORNIA 95694

**MCCANDLESS & ASSOCIATES ARCHITECTS, INC.**  
 405 1/2 Main Street, Suite 204  
 Winters, CA 95694  
 PH: (916) 837-2700  
 FAX: (916) 837-2425  
 www.mccandless.com



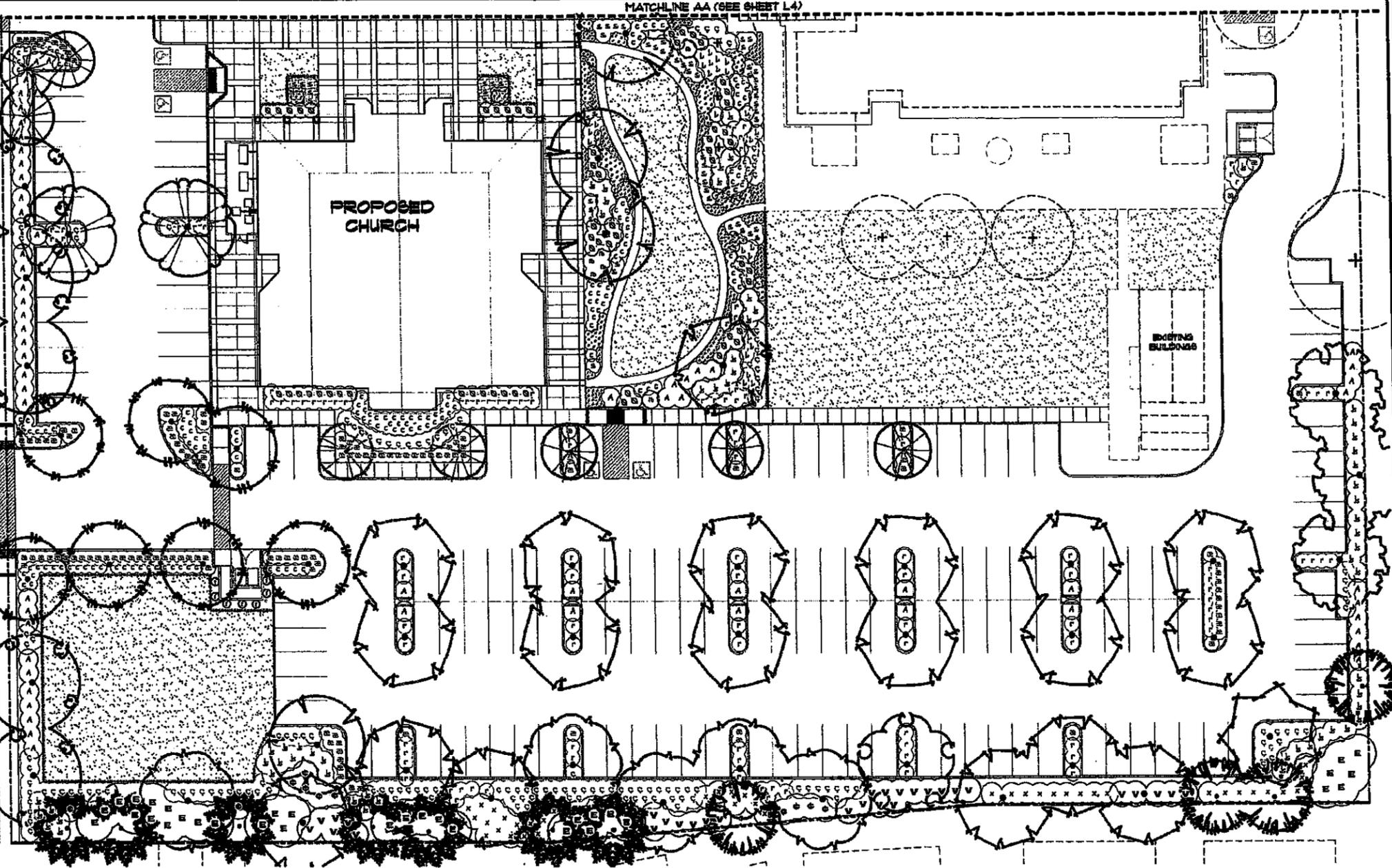
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| CHECKED | AT |



**SUMMARY TREE LIST**  
SYMBOL BOTANIC NAME/COMMON NAME

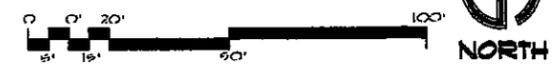
-  ACER RUBRUM 'OCTOBER GLORY' / RED MAPLE
-  CALOCEDRUS RECURRENS / INCENSE CEDAR
-  CERIS OCCIDENTALIS / WESTERN REDBUD
-  CINNAMOMUM CAMPHORA / CAMPHOR TREE
-  GINKGO BILOBA 'SAFAYOGA' / MAIDENHAIR TREE
-  JUGLANS C. 'HINDSI' / CALIF. BLACK WALNUT
-  LIBODENDRON ULPIPERA / TULIP TREE
-  OLEA EUROPEA 'WILSONI' / FRUITLESS OLIVE
-  PISTACHIA CHINENSIS / CHINESE PISTACHE
-  QUERCUS POLGLIARI / BLUE OAK
-  QUERCUS LOBATA / VALLEY OAK
-  QUERCUS BLANCA / NORTHERN RED OAK
-  QUERCUS ALBUR / COOK OAK
-  QUERCUS WULFENII / INTERIOR LIVE OAK
-  QUERCUS SENNARIENS APICOLIS / COAST REDWOOD
-  EXISTING TREE TO REMAIN



**SUMMARY SHRUB LIST & LEGEND**  
SYMBOL BOTANIC NAME/COMMON NAME

- |   |  |
|---|--|
| <ul style="list-style-type: none"> <li>■ ARCTOSTAPHYLOS X. 'EMERALD CARPET' / DW. MANZANITA</li> <li>A ARCTOSTAPHYLOS D. 'HOWARD MUMFORD' / MANZANITA</li> <li>C CALLISTEMON V. 'LITTLE JOHN' / DW. BOTTLE BRUSH</li> <li>□ CEANOTHUS 'CONCHA' / CALIFORNIA LILAC</li> <li>□ COPROSMIA P. 'VERDE VISTA' / COPROSMIA</li> <li>E ELAEAGNUS PLINGENS 'FRATLANDER' / SILVERSEED</li> <li>F FICUS PUM. A / CREEPING FIG</li> <li>B GELSEMIUM SEMPERVIRENS / CAROLINA JESSAMINE</li> <li>• MEMBRICALLIS ( MIXED EVERGREEN HYBRID ) / DAVALL</li> <li>h LAMPROLIS C. 'SEA GREEN' / LAMPER</li> <li>h JASMINUM POLYANTHUM / CLIMBING JASMINE</li> </ul> | <ul style="list-style-type: none"> <li>• KNIPHOKIA LYMBIA 'FLAMENCO' / MED HGT POKER</li> <li>L LAVANDELLA X. 'GOODWIN CREEK GRAY' / DW. LAVENDER</li> <li>□ PHORNILUM T. 'BRIZIDE DADDY' / NEW ZEALAND FLAX</li> <li>• ROSA X. 'FLOWER CARPET WHITE' / LANDSCAPE ROSE</li> <li>• RHAPHIOLEPIS I. 'HALEKINA' / DW. INDIAN HAWTHORN</li> <li>R RAPHIOLEPIS I. 'CLARA' / INDIAN HAWTHORN</li> <li>• ROSMARINUS O. 'ACAN CL. E.' / ROSEMARY</li> <li>■ PYRUS SERPILLUM 'REITER'S' / CREEPING THYME</li> <li>• SALICARIA VIOLACEA / SOLISTY GRASS</li> <li>v VERANILIA 'FINUS 'ROBUSTUM' / LAURISTINUS</li> <li>x XILOSSMA G. 'COMPACTA' / COMPACT XILOSSMA</li> </ul> |
|---|--|
-  RECYCLED TURF AREA  
 RECYCLED PLASTIC LANDSCAPE BOARD, 'EPIC PLASTICS' OR APPROXIMATELY EQUAL IN 2" x 4" SIZE ( 610 ) 200-92209

SEE SHEET L3 FOR COMPLETE PLANT LIST, LEGEND, NOTES & CALCULATIONS.



**Garth Ruffner Landscape Architect** (916) 787-2578  
4120 Douglas Blvd., Suite 305, #201, Placerville, CA 95349-5158  
Fax: 787-2377 California R.L.A. #2508

REVISIONS

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PROPOSED PROJECT:  
**ST. ANTHONY CHURCH**  
811 MAIN STREET  
WINTERS, CALIFORNIA 95694

**MCCANDLESS & ASSOCIATES ARCHITECTS, INC.**  
PH: (916) 662-9146  
FAX: (916) 662-9146  
www.mccandlessarchitects.com



**SOUTH PLANTING PLAN**

|             |               |
|-------------|---------------|
| DRAWN BY    | GR            |
| CHECKED BY  | GR            |
| DATE        | 1-2-12        |
| SCALE       | 1" = 20'-0"   |
| JOB #       | 06-204(25100) |
| SHEET       | L5            |
| OF 3 SHEETS |               |



# ATTACHMENT "G"





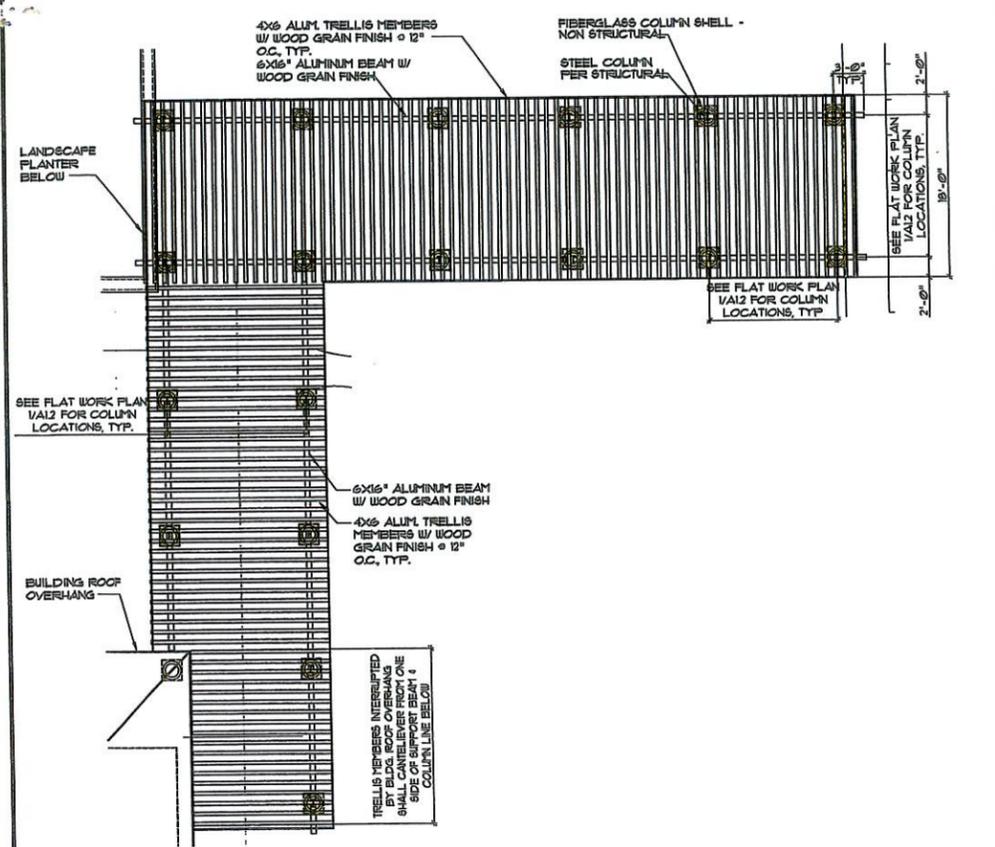




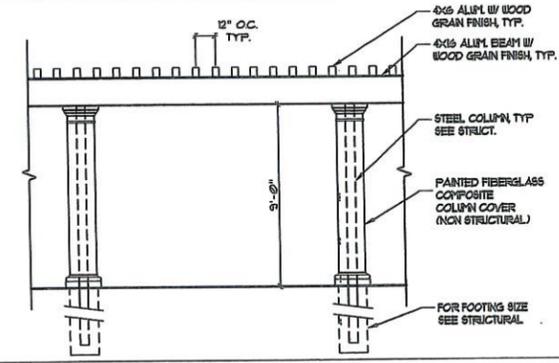


# ATTACHMENT "H"



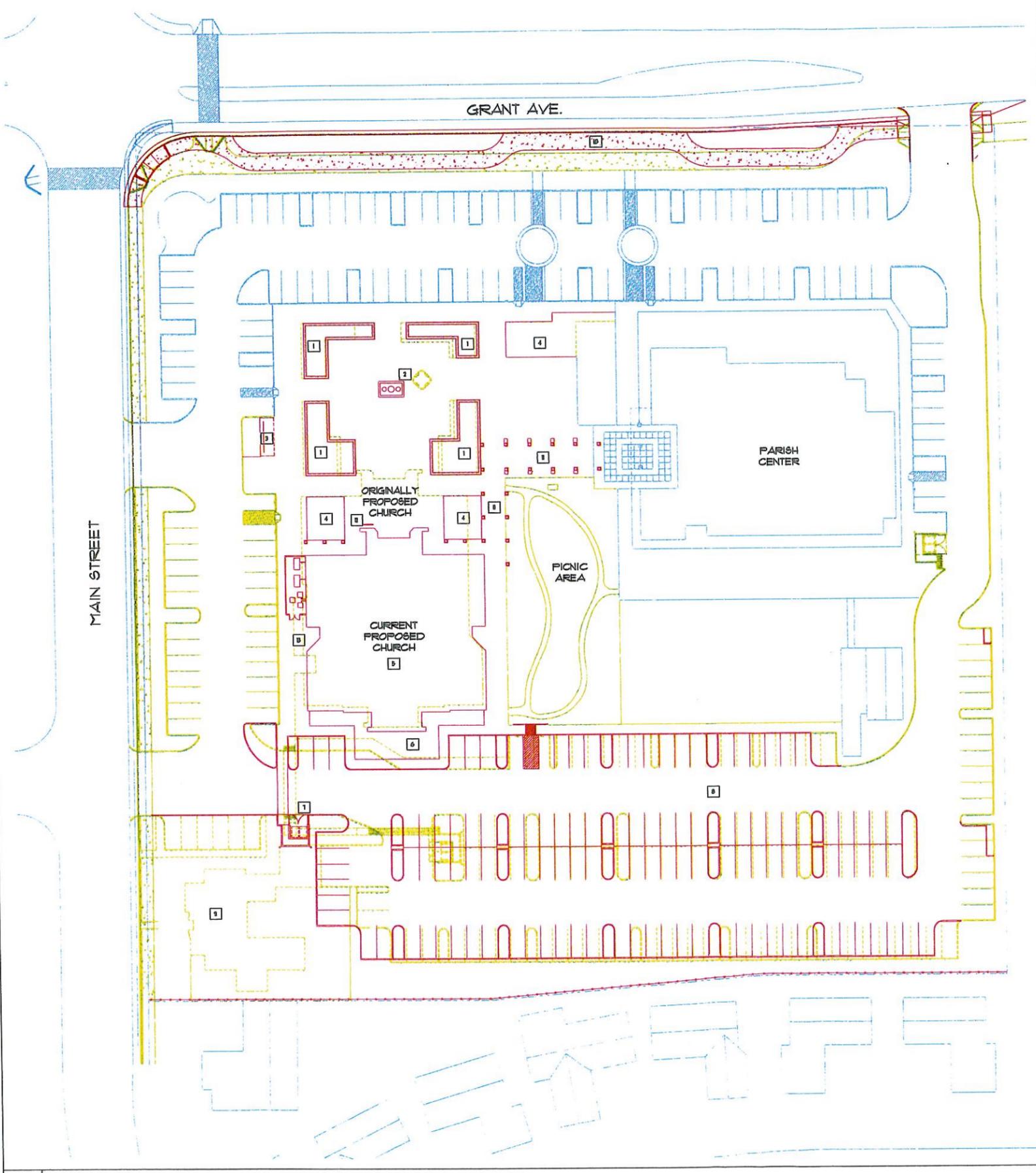


6 TRELLIS PLAN SCALE: 1/8" = 1'-0"



7 TRELLIS ELEVATION SCALE: 1/4" = 1'-0"

- KEY NOTES:**
- SOLID BLUE LINE INDICATES WHAT IS EXISTING
  - SOLID GREEN LINE INDICATES NO CHANGE TO THE ORIGINAL SUBMITTAL
  - - - DASHED GREEN LINE INDICATES CHANGES FROM THE 1ST SUBMITTAL
  - SOLID RED LINE IS THE CURRENT PROPOSED PLAN
- 1 REDESIGNED PLANTERS, RAISED THEM AND ADDED A CAP FOR SEATING
  - 2 RELOCATED AND REDESIGNED THE WATER FOUNTAIN
  - 3 ADDED BIKE RACKS
  - 4 ADDED PLANTERS ON GRADE
  - 5 DOWNSIZED CHURCH AND MOVED SLIGHTLY SOUTH ON SITE
  - 6 LARGER LANDSCAPE AREA
  - 7 RELOCATED THE TRASH ENCLOSURE
  - 8 REWORKED THE PARKING LOT ADDING 2 SPACES
  - 9 ADDED LANDSCAPE TO WHAT WAS "FUTURE RECTORY"
  - 10 MEANDERING WALK ADDED AS REQUESTED BY THE CITY
  - 11 ARBOR ADDED BETWEEN THE PARISH CENTER AND CHURCH
  - 12 NEW BELL STAND
  - 13 AREA NEEDED FOR COMPLIANCE, LANDSCAPE REMOVED FOR WALK PATH CLEARANCES



1 SITE PLAN SCALE: 1" = 30'-0"

REVISIONS

PROPOSED PROJECT:  
**ST. ANTHONY**  
 311 MAIN STREET  
 WINTERS, CALIFORNIA 95694

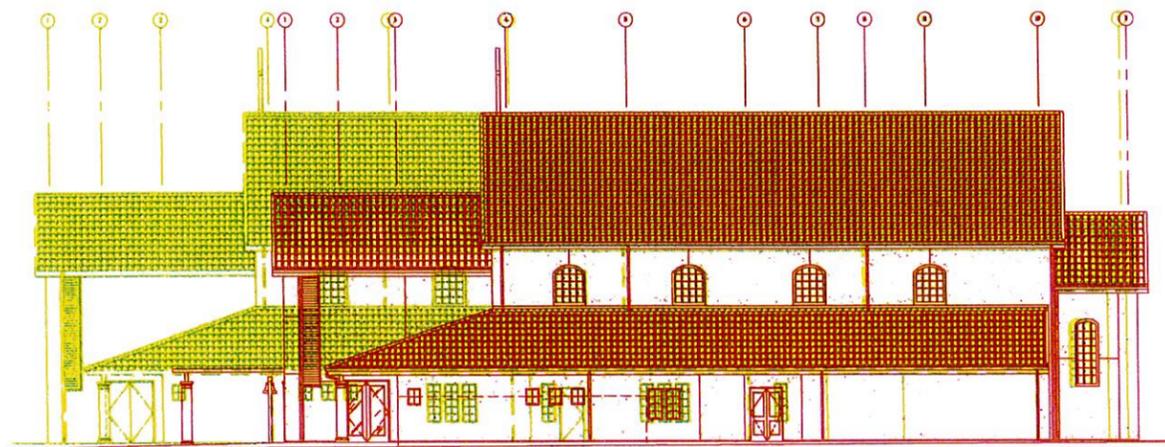
**McCANDLESS & ASSOCIATES ARCHITECTS, INC.**  
 P.O. BOX 1000, WINTERS, CALIFORNIA 95694  
 (530) 832-3400  
 www.mccandlessarchitects.com

REGISTERED ARCHITECT  
 STATE OF CALIFORNIA  
 12 95

DRAIN  
 MAA  
 CHECKED  
 MAA  
 DATE  
 07-03-2012  
 SCALE

JOB #  
 12-165  
 SHEET  
 01-1  
 OF X SHEETS

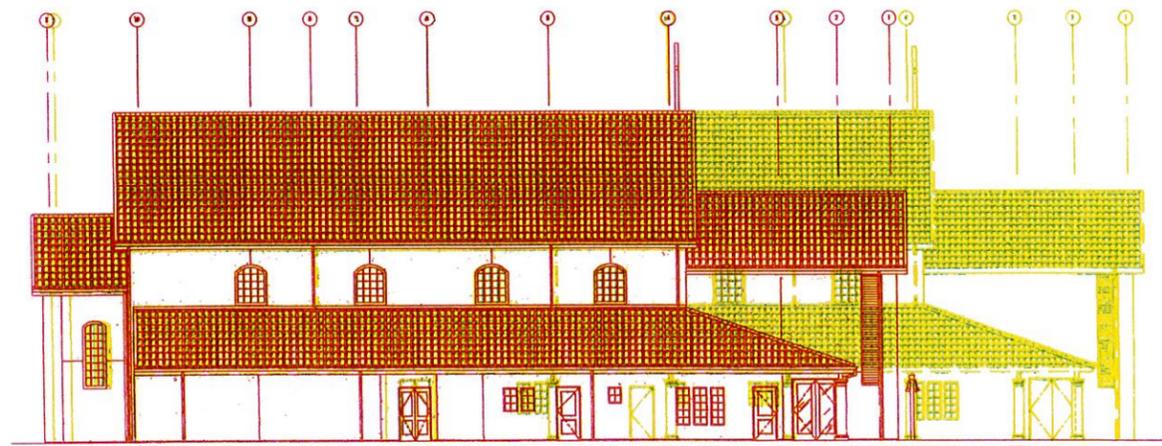




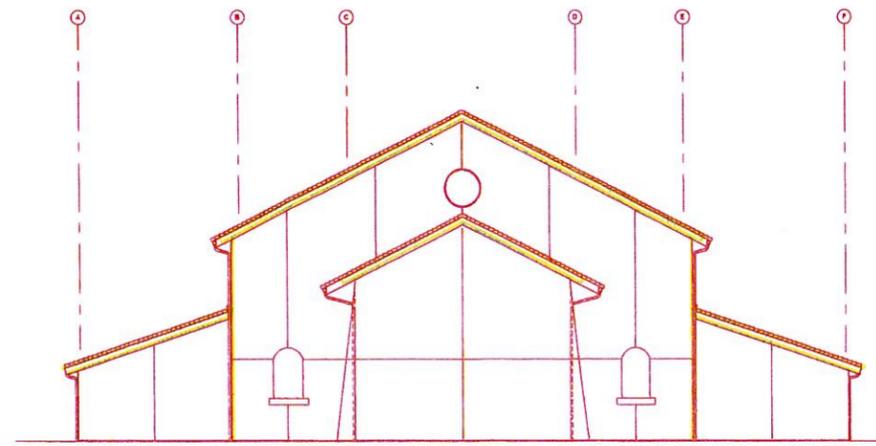
WEST ELEVATION  
SCALE: 3/32" = 1'-0"



NORTH ELEVATION  
SCALE: 3/32" = 1'-0"



EAST ELEVATION  
SCALE: 3/32" = 1'-0"



SOUTH ELEVATION  
SCALE: 3/32" = 1'-0"

| REVISIONS |
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PROPOSED PROJECT:  
**ST. ANTHONY**  
511 MAIN STREET  
WINTERS, CALIFORNIA 95694

**MCCANDLESS & ASSOCIATES ARCHITECTS, INC.**  
205 W. First Street, Suite 204  
Winters, CA 95694  
www.mccandlessarchitect.com  
PH: (916) 662-2446  
FAX: (916) 662-2448



DRAWN  
MAA  
CHECKED  
MAA  
DATE  
01-23-2012  
SCALE  
JOB #  
12-165  
SHEET  
OL-2  
OF X SHEETS



# ATTACHMENT "I"



## ENVIRONMENTAL CHECKLIST AND INITIAL STUDY

**Project Title:** St. Anthony's Church and Rectory Project

**Lead Agency:** City of Winters  
Community Development Department  
318 First Street  
Winters, CA 95694

**Lead Agency Contact:** Kate Kelly, Contract Planner  
(530) 902-1615

**Project Location:** 511 Main Street, Assessor Parcel Number APN 003-120-03

The project site is located on the west side of the City of Winters on the southeast corner of West Main Street and Grant Avenue (SR 128).

**Project Sponsor's:** Roman Catholic Bishop of Sacramento  
2110 Broadway  
Sacramento, CA 95818

Bill McCandless + Associates  
666 Dead Cat Alley  
Woodland, CA 95695

**General Plan Designation(s):** Medium Density Residential (MR)

**Zoning:** Single Family Residential 6,000 sq. ft. average minimum (R-2)

### **Existing Conditions:**

Surrounding land uses include:

North – Single Family Residential and vacant – future City Public Safety Center

West – Vacant – future single family residential

East – Waggoner Elementary School

South – Single Family Residential

**Project History:** The site development started in the late 1980's with the construction of the existing 13,235 square foot St. Anthony's Parish Community Center and associated parking areas and improvements on the north and north eastern portion of the property. A future church, rectory, and ancillary building were anticipated that that

time but were not developed. The applicant submitted a conceptual design and site plan in May 2008 which was presented to the Planning Commission for Conceptual Design Review on May 27, 2008. An application for Planned Unit Development Overlay (modified parking and height limits), Conditional Use Permit (religious institution in an R-2 zone), and Design Review (construction of non-residential building over 500 square feet) on October 16, 2008 and was deemed "Complete" on January 16, 2009 for the purposes of this CEQA analysis.

**Previous Relevant Environmental Analysis:**

*Winters Highlands, Callahan Estates, Ogando-Hudson & Creekside Estates Traffic Impact Study, Prepared for the City of Winters, Prepared by Grandy & Associates/Fehr and Peers Associates, Inc, July 2004*

**Description of the Project:**

This project proposes to construct a 13,813 square foot, 700 seat church and a 2,900 square foot rectory with a two car garage at the southeast corner of Grant Avenue (State Route 128) and West Main Street. The property is 5.56+/- acres and is currently developed with a 13,235+/- square foot community center, 827 +/- square foot auxiliary building, a storage shed, and associated parking and landscaping. The parcel is generally square with frontage on Grant Avenue, and West Main Street. The frontage along Grant Avenue is 478+/- feet in length and the frontage on West Main Street is approximately 472+/- feet. The parcel abuts Waggoner Elementary School to the east and a residential neighborhood to the south.

**Land Use Changes**

The parcel is currently designated Medium Density Residential (MR) in the General Plan and is zoned Single Family Residential 6,000 sq. ft. average minimum (R-2). The applicant has requested a Planned Development Overlay to modify the height restriction and parking requirements. The maximum height in the R-2 zone is 30 feet. The proposed church is 41' 4" tall at the peak of the roof and 51' tall at the top of the roof mounted cross.

The total onsite parking required for the site developed with the existing community center, existing auxiliary building, proposed church and proposed rectory would be 271 spaces per the parking standards in Section 17.72.020 of the City of Winters Municipal Code. The proposed project provides 236 spaces which amounts to 87% of the required spaces.

**Proposed Buildings**

**Church** is a 13,813 square foot Spanish Colonial style building which would seat 700 and includes space for a lobby, acolytes-altar servers, vestry-dressing room, confessionals, restrooms, arts-eucharist preparation, storage, sacristy-religious storage, sanctuary-alter, choir, sound system, electrical-mechanical service, music room,

janitorial, and bride's room. The building is single story and is located in the west central portion of the parcel. Due to the roofline, the building is 41' 4" tall with total height of 51' including the roof mounted cross.

**Rectory** - a 2,900 square foot three bedroom, four bath residential unit which also has a study, living/dining areas, kitchen and an attached two car garage. The rectory is proposed for the southeast corner of the parcel with frontage on West Main Street and the garage located on the east side which would be accessed via the site's interior roadway and parking areas.

#### Sewer Conveyance

Sanitary sewer service for the church is proposed to be provided via an 8" main which would be constructed across the central portion of the site and would connect to an existing 8 inch municipal sanitary sewer line located at the western boundary of the property. Sanitary sewer service for the rectory would be provided via connections to the existing 8 inch municipal sanitary sewer line located at the western boundary of the property.

#### Sewer Treatment

The City's Wastewater Treatment Plant (WWTP) has a capacity of 0.92 million gallons per day (mgd). Space remains for approximately 600 additional residential hook-ups. The City's recent project approvals exceed this amount and expansion of the plant is planned. The Phase 2 expansion will bring the capacity to between 1.2 and 1.6 mgd.

#### Water Conveyance

Municipal water is proposed to be provided to the property via the existing 12 inch water main on the north side of the property and the 10 inch water main on the west side of the property, and the 6 inch water main on the south side of the property. Water would be conveyed within the property via 8, 6, 4 and 3 inch lines which are proposed to connect to the municipal lines on the north and west south sides of the property.

#### Drainage Conveyance

Storm water is proposed to be collected on site via a series of grated intakes in parking and driveway areas conveyed off site via a east-west running storm drainage line to the western edge of the property where it would connected to an existing 24 inch municipal storm water drain line located in West Main Street.

#### Off-Site Infrastructure

The project would be required to fund and construct off-site improvements necessary to support the development. Such improvements would include, but not be limited to traffic control, water lines, sewer lines and storm drainage lines. To the extent that acquisition or subsequent CEQA clearance is necessary for such work, that would be the responsibility of the developer.

### Flooding

The project does not fall within the City's General Plan Flood Overlay Area. The project site lies in FEMA Flood Zone X (un-shaded) based on the FEMA Flood Insurance Rate Map (map revised November 20, 1998, Community-Panel Number 060425 0001 C). Zone X (un-shaded) is a flood insurance rate zone assigned to property that is determined to be outside the 500-year floodplain.

### Architecture

The new church and rectory are in a Spanish Colonial style which is reflective of the community center building which was constructed on the property in the late 1980s. The new church and rectory show a simplified mission style. Materials will be painted stucco, stained wood and red clay tile roofing to match the existing community center.

### Entitlements

The project requires the following approvals from the City:

- Planned Development Overlay (building height and parking requirements)
- Conditional Use Permit for Church (religious institution in R-2 zone)
- Design Review (construction of nonresidential building/structures exceeding 500 square feet)

**Other public agencies whose approval may be required (e.g., permits, financing approval, or participation agreement):** Encroachment permit from Caltrans for project interface with State Highway 128/Grant Avenue.

**Other Project Assumptions:** The Initial Study assumes compliance with all applicable State, Federal, and local codes and regulations including, but not limited to, City of Winters Improvement Standards, the California Building Code, the State Health and Safety Code, and the State Public Resources Code.

**Technical Studies:** The following technical and other site-specific studies and reports have been prepared for the project and are relied upon in this analysis:

**Cultural Resources Survey** – *Far Western Anthropological Research Group (August 2008)* Record search and field survey. No cultural resources were identified in either the literature for this location or on the surface of the property.

**Geotechnical Engineering Report** – Laver L. Roper and Associates ( July 1988) and *Raney Geotechnical (October 2008)*

The site and soil conditions were investigated in 1988 for the initial proposed development which included the community center, church, rectory, and auxiliary building by Laver Roper. Raney Geotechnical reviewed the Laver Roper report, conducted a site visit, obtained soil samples, conducted an Atterberg Limits test to assess plasticity properties and provided recommendations for the site preparation and construction of the proposed project.

**Noise Analysis for the St. Anthony's Church Expansion in Winters, California – Michael Brandman Associates (June 2008)**

This report provides the results of noise monitoring and modeling performed for the proposed project. The report identifies areas of noise impact resulting from expected on-site operational noise generators such as church activities, roof top HVAC units, parking lot use and delivery trucks.

**Air Quality Analysis for the St. Anthony's Church Expansion in Winters, California – Michael Brandman Associates (June 2008)**

The analysis was prepared to evaluate potential air quality impacts from the proposed project and to address potential Global Climate Change impacts related to the proposed project.

These reports are on file at the Community Development Department at the City of Winters.

**ENVIRONMENTAL FACTORS POTENTIALLY AFFECTED:**

The environmental factors checked below potentially would be significantly affected by this project, as indicated by the checklist on the following pages.

- |   |  |
|---|--|
| <input checked="" type="checkbox"/> Aesthetics            | <input type="checkbox"/> Mineral Resources                             |
| <input type="checkbox"/> Agricultural Resources           | <input checked="" type="checkbox"/> Noise                              |
| <input checked="" type="checkbox"/> Air Quality           | <input type="checkbox"/> Population and Housing                        |
| <input checked="" type="checkbox"/> Biological Resources  | <input type="checkbox"/> Public Services                               |
| <input checked="" type="checkbox"/> Cultural Resources    | <input type="checkbox"/> Recreation                                    |
| <input checked="" type="checkbox"/> Geology and Soils     | <input type="checkbox"/> Transportation/Traffic                        |
| <input type="checkbox"/> Hazards and Hazardous Materials  | <input checked="" type="checkbox"/> Utilities and Service Systems      |
| <input type="checkbox"/> Hydrology/Water Quality          | <input checked="" type="checkbox"/> Mandatory Findings of Significance |
| <input checked="" type="checkbox"/> Land Use and Planning | <input type="checkbox"/> None Identified                               |

**DETERMINATION:**

On the basis of this initial evaluation:

- I find that the Proposed Project **COULD NOT** have a significant effect on the environment, and a **NEGATIVE DECLARATION** will be prepared.
- I find that although the Proposed Project could have a significant effect on the environment, there will not be a significant effect in this case because revisions in the project have been made by or agreed to by the project proponent. A **MITIGATED NEGATIVE DECLARATION** will be prepared.
- I find that the Proposed Project **MAY** have a significant effect on the environment, and an **ENVIRONMENTAL IMPACT REPORT** is required.
- I find that the Proposed Project **MAY** have a "potentially significant impact" or "potentially significant unless mitigated" impact on the environment, but at least one effect 1) has been adequately analyzed in an earlier document pursuant to applicable legal standards, and 2) has been addressed by mitigation measures based on the earlier analysis described in the attached sheets. An **ENVIRONMENTAL IMPACT REPORT** is required, but it must analyze only the effects that remain to be addressed.
- I find that although the Proposed Project could have a significant effect on the environment, because all potentially significant effects (a) have been analyzed adequately in an earlier EIR or **NEGATIVE DECLARATION** pursuant to applicable standards, and (b) have been avoided or mitigated pursuant to the earlier EIR or **NEGATIVE DECLARATION**, including revisions or mitigation measures that are imposed upon the Proposed Project. Nothing further is required.

\_\_\_\_\_  
Signature

Kate Kelly, Contract Planner  
Printed Name

February 12, 2009  
Date

Community Development Department  
Lead Agency

## ENVIRONMENTAL CHECKLIST

### Introduction

Following is the environmental checklist form presented in Appendix G of the CEQA Guidelines. The checklist form is used to describe the impacts of the Proposed Project. A discussion follows each environmental issue identified in the checklist. Included in each discussion are project-specific mitigation measures recommended as appropriate as part of the Proposed Project.

For this checklist, the following designations are used:

**Potentially Significant Impact:** An impact that could be significant, and for which no mitigation has been identified. If any potentially significant impacts are identified, an EIR must be prepared.

**Potentially Significant Unless Mitigation Incorporated:** An impact that requires mitigation to reduce the impact to a less than significant level.

**Less Than Significant Impact:** Any impact that would not be considered significant under CEQA relative to existing standards.

**No Impact:** The project would not have any impact.

### Instructions

1. A brief evaluation is required for all answers except "No Impact" answers that are adequately supported by the information sources a lead agency cites in the parentheses following each question. A "No Impact" answer is adequately supported if the referenced information sources show that the impact simply does not apply to projects like the one involved (e.g. the project falls outside a fault rupture zone). A "No Impact" answer should be explained where it is based on project-specific factors as well as general standards (e.g. the project will not expose sensitive receptors to pollutants, based on a project-specific screening analysis).
2. All answers must take account of the whole action involved, including off-site as well as on-site, cumulative as well as project-level, indirect as well as direct, and construction as well as operational impacts.
3. Once the lead agency has determined that a particular physical impact may occur, then the checklist answers must indicate whether the impact is potentially significant, potentially significant unless mitigation is incorporated, or less than significant. "Potentially significant impact" is appropriate if there is substantial evidence that an effect may be significant. If there are one or more "Potentially Significant Impact" entries when the determination is made, an EIR is required.

4. **“Potentially Significant Unless Mitigation Incorporated” means “Less Than Significant With Mitigation Incorporated”.** It applies where incorporation of mitigation measures has reduced as effect from **“Potentially Significant Impact”** to a **“Less Than Significant Impact”**. The lead agency must describe the mitigation measures, and briefly explain how they reduce the effect to a less than significant level (mitigation measures from earlier analyses may be cross-referenced).
5. Earlier analyses may be used where, pursuant to tiering, a program EIR, or other CEQA process, an effect has been adequately analyzed in an earlier EIR or negative declaration (Section 15063(c)(3)(D)). In this case, a brief discussion should identify the following:
  - a. **Earlier Analysis Used – Identify and state where available for review.**
  - b. **Impacts Adequately Addressed – Identify which effects from the above checklist were within the scope of and adequately addressed in an earlier document pursuant to applicable legal standards, and state whether such effects were addressed by mitigation measures based on the earlier analysis.**
  - c. **Mitigation Measures – For effects that are “Potentially Significant Unless Mitigation Incorporated” describe the mitigation measures that were incorporated or refined from the earlier document and the extent to which they address site-specific conditions for the project.**
6. **Lead agencies are encouraged to incorporate into the checklist references to information sources for potential impacts (e.g. general plans, zoning ordinances). Reference to a previously prepared or outside document should, where appropriate, include a reference to the page or pages where the statement is substantiated.**
7. **Supporting Information Sources in the form of a source list should be attached, and other sources used or individuals contacted should be cited in the discussion.**
8. **This is only a suggested form, and lead agencies are free to use different formats; however, lead agencies should normally address the questions from this checklist that are relevant to a project’s environmental effects in whatever format is selected.**
9. **The explanation of each issue area should identify: a) the significance criteria or threshold, if any, used to evaluate each question; and b) the mitigation measures identified, if any, to reduce the impact to less than significant.**

| Issues   | Potentially Significant Impact | Potentially Significant Unless Mitigation Incorporated | Less-Than-Significant Impact        | No Impact                           |
|--|--------------------------------|--|-------------------------------------|-------------------------------------|
| <b>1. AESTHETICS.</b><br><i>Would the project:</i>   |                                |  |                                     |                                     |
| a. Have a substantial adverse effect on a scenic vista?  | <input type="checkbox"/>       | <input type="checkbox"/>                               | <input checked="" type="checkbox"/> | <input type="checkbox"/>            |
| b. Substantially damage scenic resources, including, but not limited to, trees, rock outcroppings, and historic buildings within a State scenic highway? | <input type="checkbox"/>       | <input type="checkbox"/>                               | <input type="checkbox"/>            | <input checked="" type="checkbox"/> |
| c. Substantially degrade the existing visual character or quality of the site and its surroundings?  | <input type="checkbox"/>       | <input checked="" type="checkbox"/>                    | <input type="checkbox"/>            | <input type="checkbox"/>            |
| d. Create a new source of substantial light or glare, which would adversely affect day or nighttime views in the area?                                   | <input type="checkbox"/>       | <input checked="" type="checkbox"/>                    | <input type="checkbox"/>            | <input type="checkbox"/>            |

**Discussion**

- a. The proposed project would change the visual characteristics of the project site, however, this site is planned for urban development. Existing and planned residential and public/quasi-public uses surrounds the site. For these reasons, the proposed project would not substantially or adversely affect views of a scenic vista, and this impact would be less than significant.
  
- b. The proposed project site does not contain any protected scenic resources. The adjoining roadways are not listed or designated as a "scenic highway" and are not designated as scenic resources by the General Plan. As such, there would be no impact.
  
- c. The proposed project would not significantly degrade the visual surroundings of the area. The project site is located adjacent to existing residential development to the north and south, an existing elementary school to the east. A residential subdivision has been approved for the vacant parcel located across street from the proposed project at the southeast corner of West Main and Grant Avenue. The City's new 36,500 +/- square foot Public Safety Center which would house both the Fire Department and the Police Department has been approved for the vacant parcels across the street from the proposed project on the northwest corner of West Main and Grant. The main Public Safety Center building will be 42 feet tall and the project also includes a 140 foot tall communication tower.  
  
The General Plan anticipates that the proposed church and rectory project site would develop at a residential density of 5.4 to 8.8 dwelling units to an acre which would result in over 100,000 square feet of residential development based on a minimum of 45% lot coverage per 17.56.010 of the Winters Municipal Code. The proposed project, including the existing community center and auxiliary

building, would result in 30,775 square feet of public/quasi-public development with remainder of the site improved with landscaped outdoor areas and parking. This proposed site density is considerably less than that of residential development. The development of the site and the proposed church are subject to design review approval by the City of Winters to for consistency with the City's Design Guidelines, which are intended to ensure that new development is compatible with the City's small-town heritage (see Section 9, Land Use and Planning). With implementation of Mitigation Measure Land Use 1 requiring design review of the project, the change in visual character would be a less than significant impact.

- d. The proposed project would provide additional light and glare in the area. If unshielded, lighting can spill onto adjacent projects, and disturb adjacent residential uses.

The proposed church would be constructed with exterior materials common to residential and public/quasi-public development in the area. Architectural materials include painted stucco, stained wood and red clay tile roofing. The proposed project buildings do not include large glass walls or highly reflective exteriors. Therefore, the proposed project would not produce substantial glare. With the applicant's agreement to accept and implement the following mitigation measure, lighting impacts would be reduced to a less than significant level, because light would be focused downward. Therefore, spillover onto other properties would not occur, and the amount of light visible from offsite would be minimized.

***Mitigation Measure Aesthetics 1 – Outdoor light fixtures shall be low-intensity, shielded and/or directed away from adjacent areas and the night sky. All light fixtures shall be installed and shielded in such a manner that no light rays are emitted from the fixture at angles above the horizontal plane. Lighting plans with certification that adjacent areas will not be adversely affected and that offsite illumination will not exceed 2-foot candles shall be submitted to the City for review and approval as part of improvement plans.***

***Prior to issuance of a building permit, the applicant shall submit a photometric and proposed lighting plan for the project to the satisfaction of the Community Development Department to ensure no spillover light and glare onto adjoining properties.***

| Issues  | Potentially Significant Impact | Potentially Significant Unless Mitigation Incorporated | Less-Than-Significant Impact | No Impact |
|---|--------------------------------|--|------------------------------|-----------|
| <b>2. AGRICULTURE RESOURCES:</b><br><i>In determining whether impacts to agricultural resources are significant environmental effects, lead agencies may refer to the California Agricultural Land Evaluation and Site Assessment Model (1997) prepared by the California Department of Conservation as an optional model to use in assessing impacts on agriculture and farmland. Would the project:</i> |                                |  |                              |           |
| a. Convert Prime Farmland, Unique Farmland, or Farmland of Statewide Importance (Farmland) as shown on the maps prepared pursuant to the Farmland Mapping and Monitoring Program of the California Resources Agency, to non-agricultural use?   | □                              | □  | ■                            | □         |
| b. Conflict with existing zoning for agricultural use, or a Williamson Act contract?  | □                              | □  | □                            | ■         |
| c. Involve other changes in the existing environment which, due to their location or nature, could result in loss of Farmland, to non-agricultural use?   | □                              | □  | □                            | ■         |

**Discussion**

- a. The project site is not designated as Prime Farmlands, Unique Farmlands, or Farmlands of Local Importance on the City's Important Farmlands Map (1992 General Plan Background Report, Figure VIII-2). The Yolo County Important Farmland Map (California Department of Conservation, 2006) designates the project site as Urban and Built-Up Land. This is a less than significant impact.
- b. No part of the project site is under a Williamson Act contract nor immediately adjacent to any lands under Williamson Act contract. In addition, the project site is not located immediately adjacent to any lands zoned for agricultural uses. Therefore, there would be no impact on Williamson Contract land or other agriculturally zoned land.
- c. Development of the property will have no impact on the conversion of other properties to non-agricultural uses or loss of farmland in general. The project site is located in an urbanized area and is not adjacent to actively producing agricultural or farmland. The subject site is an infill property and development of a church and rectory on this property has been anticipated for at least 20 years. For this reason, no impact will occur in this category.

| Issues  | Potentially Significant Impact | Potentially Significant Unless Mitigation Incorporated | Less-Than-Significant Impact        | No Impact                |
|---|--------------------------------|--|-------------------------------------|--------------------------|
| <b>3. AIR QUALITY.</b>  |                                |  |                                     |                          |
| <i>Where available, the significance criteria established by the applicable air quality management or air pollution control district may be relied upon to make the following determinations. Would the project:</i>  |                                |  |                                     |                          |
| a. Conflict with or obstruct implementation of the applicable air quality plan?   | <input type="checkbox"/>       | <input type="checkbox"/>                               | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| b. Violate any air quality standard or contribute substantially to an existing or projected air quality violation?  | <input type="checkbox"/>       | <input checked="" type="checkbox"/>                    | <input type="checkbox"/>            | <input type="checkbox"/> |
| c. Result in a cumulatively considerable net increase of any criteria pollutant for which the project region is non-attainment under an applicable federal or state ambient air quality standard (including releasing emissions which exceed quantitative thresholds for ozone precursors)? | <input type="checkbox"/>       | <input type="checkbox"/>                               | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| d. Expose sensitive receptors to substantial pollutant concentrations?  | <input type="checkbox"/>       | <input checked="" type="checkbox"/>                    | <input type="checkbox"/>            | <input type="checkbox"/> |
| e. Create objectionable odors affecting a substantial number of people?   | <input type="checkbox"/>       | <input type="checkbox"/>                               | <input checked="" type="checkbox"/> | <input type="checkbox"/> |

### **Discussion**

The Yolo Solano Air Quality Management District (YSAQMD) is responsible for developing rules that regulate stationary sources, area sources, and certain mobile sources. In addition, the YSAQMD establishes permitting requirements for stationary sources, enforces air quality rules, and maintains air quality monitoring stations in Yolo County and the Solano County. The YSAQMD is responsible for developing and updating the state attainment plans and triennial assessments.

Air Quality Analysis for the proposed project was prepared by Michael Brandman Associates (June 2008) to evaluate whether the expected air pollutant emissions generated from the proposed project would cause significant impact to air resources in the project area. Their assessment was conducted within the context of CEQA and the methodology followed the Handbook for Assessing and Mitigating Air Quality Impacts (CEQA Handbook) prepared by the Yolo-Solano Air Quality Management District (YSAQMD). The CEQA Handbook sets forth recommended thresholds of significance, screening criteria, analysis methodologies, and provides guidance on mitigating significant impacts. In addition to the YSAQMD thresholds, their analysis addressed the qualitative questions from Appendix G of the State CEQA Guidelines and provides a Global Climate Change threshold and determination.

- a. The Yolo-Solano Air Quality Management District is currently a non-attainment for ozone (State and Federal ambient standards) and Particulate Matter (State ambient standards). While air quality plans exist for ozone, none exists (or is currently required) for PM<sub>10</sub>.

Based on consistency with the regional air plan, the YSAQMD CEQA guidance provides that a development project would have a cumulatively significant impact with respect to a non-attainment pollutant if the project requires a change in the existing land use designation (i.e., general plan amendment), and projected emissions of ozone precursors for the proposed project are greater than the emissions anticipated for the site if developed under the existing land use designation. The project site is designated as Medium Density Residential (MR) by the City of Winters General Plan, and is currently zoned Single-Family Residential (R-2). Per the Land Use / Circulation Diagrams and Standards Section of the General Plan, the MR designation provides for, "single-family detached and attached homes, public and quasi-public uses, and similar and compatible uses." Churches are considered a quasi-public use under the General Plan. Therefore, construction and operation of the project is consistent with the General Plan, and by extension air quality attainment plans and the state implementation plan. As a result, the impact would be less than significant.

- b. The YSAQMD has developed screening sizes for various land uses to identify projects that would likely exceed the CEQA Handbook's thresholds for criteria pollutants. Table 2 of the CEQA Handbook contains the land use categories and relative sizes reasonably expected to exceed the thresholds for 2007 and 2010. The project screening sizes listed in the table are larger in 2010 than 2007 due to the cleaner emission factors for future years. This project is expected to be constructed in 2009 and operational by 2010. The table lists the square footage for places of worship that may exceed YSAQMD thresholds for ROG, NOx and PM<sub>10</sub> at 440,000 square feet for 2007 and 560,000 for 2010. The proposed St. Anthony's expansion at 16,713 square feet is well below the screening sizes listed in the CEQA Handbook.

**CO Screening Criteria** - The YSAQMD's CEQA Handbook states that a project can be said to have the potential to create a violation of the CO standard (and thus need a local CO hotspot analysis) if either of the following criteria are true for any intersection affected by the project:

- A traffic study for the project indicates that the peak-hour Level of Service (LOS) on one or more streets or at one or more intersections in the project vicinity will be reduced to an unacceptable LOS (typically LOS E or F); or
- A traffic study indicates that the project will substantially worsen an already existing peak-hour LOS F on one or more streets or at one or more intersections in the project vicinity. "Substantially worsen" includes situations where delay would increase by 10 seconds or more when

project-generated traffic is included.

A traffic study was not prepared for this project. This project will contribute to trips in the project area; however, at the Institute of Transportation Engineers' (ITE) trip generation rates for 'Place of Worship' and 'Single-Family Housing', the project would only contribute approximately 112 trips per day for the church expansion and 10 trips per day for the Rectory. This low level of trip generation is not likely to impact the LOS of nearby intersections. Therefore, the project impact is less than screening criteria established by YSAQMD and no further analysis is required. Nonetheless, for purposes of consistency the City is imposing the same air quality mitigations measures on this project as it has on other recently projects approved by the City. Additionally it should be pointed out that General Plan Policy VI.E.6 requires controls for construction-related dust.

With the applicant's agreement to accept and implement the following mitigation measure, NO<sub>x</sub> emissions would be minimized and this impact would be held to a less than significant level.

***Mitigation Measure Air 1***

- a. *Construction equipment exhaust emissions shall not exceed District Rule 2-11 Visible Emission limitations.*
- b. *Construction equipment shall minimize idling time to 5 minutes or less. Catalyst and filtration technologies shall be incorporated where feasible.*
- c. *The prime contractor shall submit to the District a comprehensive inventory (i.e. make, model, year, emission rating) of all the heavy-duty off-road equipment (50 horsepower or greater) that will be used an aggregate of 40 or more hours for the construction project. District personnel, with assistance from the California Air Resources Board, will conduct initial Visible Emission Evaluations of all heavy-duty equipment on the inventory list.*

*An enforcement plan shall be established to weekly evaluate project-related on-and-off-road heavy-duty vehicle engine emission opacities, using standards as defined in California Code of Regulations, Title 13, Sections 2180 - 2194. An Environmental Coordinator, CARB-certified to perform Visible Emissions Evaluations (VEE), shall routinely evaluate project related off-road and heavy duty on-road equipment emissions for compliance with this requirement. Operators of vehicles and equipment found to exceed opacity limits will be notified and the equipment must be repaired within 72 hours.*

*Construction contracts shall stipulate that at least 20% of the heavy-duty off-road equipment included in the inventory be powered by CARB certified off-road engines, as follows:*

|                        |                               |
|------------------------|-------------------------------|
| <i>175 hp - 750 hp</i> | <i>1996 and newer engines</i> |
| <i>100 hp - 174 hp</i> | <i>1997 and newer engines</i> |
| <i>50 hp- 99 hp</i>    | <i>1998 and newer engines</i> |

*In lieu of or in addition to this requirement, the applicant may use other measures to reduce particulate matter and nitrogen oxide emissions from project*

*construction through the use of emulsified diesel fuel and or particulate matter traps. These alternative measures, if proposed, shall be developed in consultation with District staff.*

**With the applicant's agreement to accept and implement the following mitigation measure, PM<sub>10</sub> emissions would be minimized and this impact would be held to a less than significant level.**

***Mitigation Measure Air 2***

- a. *Nontoxic soil stabilizers according to manufacturer's specifications shall be applied to all inactive construction areas (previously graded areas inactive for ten days or more).*
  - b. *Ground cover shall be reestablished in disturbed areas quickly.*
  - c. *Active construction sites shall be watered at least three times daily to avoid visible dust plumes.*
  - d. *Paving, applying water three times daily, or applying (non-toxic) soil stabilizers shall occur on all unpaved access roads, parking areas and staging areas at construction sites*
  - e. *Enclosing, covering, watering daily, or applying non-toxic soil binders to exposed stockpiles (dirt, sand, etc.) shall occur.*
  - f. *A speed limit of 15 MPH for equipment and vehicles operated on unpaved areas shall be enforced.*
  - g. *All vehicles hauling dirt, sand, soil, or other loose materials shall be covered or shall be maintained at least two feet of freeboard.*
  - h. *Streets shall be swept at the end of the day if visible soil material is carried onto adjacent public paved roads.*
- c. **Project traffic emissions would have an effect on air quality outside the project vicinity. Trips to and from the project and area sources associated with the proposed uses would result in air pollutant emissions within the air basin. The YSAQMD considers any project that would individually have a significant air quality impact to also have a significant cumulative impact. As described under Questions A, B, and D, the project would not individually exceed any project-level threshold. As a result, project regional (operational) air quality impacts would be less than significant.**
- d. **Construction activities such as clearing, excavation and grading operations, construction vehicle traffic and wind blowing over exposed earth would generate exhaust emissions and fugitive particulate matter emissions that would temporarily affect local air quality for adjacent land uses.**

**Localized air impacts occur when a sensitive receptor is exposed to substantial air pollution, often as a result of land use conflicts. Unlike ozone precursors,**

which have a regional air quality impact, emissions of CO, TACs, odor and dust affect the land uses in close vicinity of the emitting source. Two situations have the potential to cause localized impacts to sensitive receptors:

- 1) A source of localized air pollutants is proposed to be located near existing or planned sensitive receptors, or
- 2) A sensitive receptor land use is proposed near an existing or planned source of localized air pollutants.

The project will construct a quasi-public facility that is compatible with the adjacent residential and other quasi public land uses. As discussed under Question B, the project will likely not affect the LOS of adjacent intersections. Therefore, the project does not have the potential to contribute to a CO Hotspot. Churches are not a known source of TACs or objectionable odors.

Therefore, the operation of the project will not expose nearby sensitive receptors to unacceptable levels of TACs, CO or odors, or substantially contribute to air-related sensitive receptor impacts. During construction, due to the short duration, low number of diesel vehicles and distance between equipment and nearby receptors, health risks from construction emissions of diesel particulate would be a less than significant impact. The mitigation requirement contained in Mitigation Measure Air 1 would mitigate the dust generated from construction of the project to a less than significant impact.

- d. During construction the various diesel-powered vehicles and equipment in use on the site would create odors. These odors are temporary and not likely to be noticeable much beyond the project boundaries. The potential for diesel odors impacts is less than significant. As discussed above, churches are not known sources of objectionable odors.

## **CLIMATE CHANGE ANALYSIS**

### **Regulatory History**

There has been significant legislative activity regarding global climate change and greenhouse gases in California. Although it was not originally intended to reduce greenhouse gases, the California Code of Regulations Title 24 Part 6: California's Energy Efficiency Standards for Residential and Nonresidential Buildings, was first adopted in 1978 in response to a legislative mandate to reduce California's energy consumption. The standards are updated periodically to allow consideration and possible incorporation of new energy efficiency technologies and methods. The latest amendments were made in October 2005 and currently require new homes to use half the energy they used only a decade ago. Energy efficient buildings require less electricity, and electricity production by fossil fuels results in greenhouse gas emissions. Therefore, increased energy efficiency results in decreased greenhouse gas emissions. California Assembly Bill 1493 (Pavley), enacted on July 22, 2002, required the CARB to develop and adopt regulations that reduce greenhouse gases emitted by passenger vehicles and light duty trucks.

Regulations adopted by the California Air Resources Board (CARB) would apply to 2009 and later model year vehicles. The CARB estimates that the regulation would reduce climate change emissions from the light-duty passenger vehicle fleet by an estimated 18 percent in 2020 and by 27 percent in 2030.

California Governor Arnold Schwarzenegger announced on June 1, 2005, through Executive Order S-3-05, the following greenhouse gas emission reduction targets:

- 1) by 2010, reduce greenhouse gas emissions to 2000 levels;
- 2) by 2020, reduce greenhouse gas emissions to 1990 levels; and
- 3) by 2050, reduce greenhouse gas emissions to 80 percent below 1990 levels (CA 2005).

To meet these targets, the Governor directed the Secretary of the Cal EPA to lead a California Climate Action Team (CAT) made up of representatives from the Business, Transportation and Housing Agency; the Department of Food and Agriculture; the Resources Agency; the Air Resources Board; the Energy Commission; and the Public Utilities Commission. The CAT's Report to the Governor in 2006 contains recommendations and strategies to help ensure the targets in Executive Order S-3-05 are met (CAT 2006).

The 2006 CAT Report contains baseline emissions as estimated by the CARB and the California Energy Commission. The emission reduction strategies reduce greenhouse gas emissions to the targets contained in AB 32.

Also in 2006, the California State Legislature adopted AB 32, the California Global Warming Solutions Act of 2006, which charged the CARB to develop regulations on how the state would address global climate change. AB 32 focuses on reducing greenhouse gas emissions in California. Greenhouse gases, as defined under AB 32, include carbon dioxide, methane, nitrous oxide, hydrofluorocarbons, perfluorocarbons, and sulfur hexafluoride. AB 32 requires that greenhouse gases emitted in California be reduced to 1990 levels by the year 2020. CARB is the state agency charged with monitoring and regulating sources of emissions of greenhouse gases that cause global warming in order to reduce emissions of greenhouse gases. AB 32 requires that by January 1, 2008, CARB must determine what the statewide greenhouse gas emissions level was in 1990, and it must approve a statewide greenhouse gas emissions limit so it may be applied to the 2020 benchmark. CARB adopted the 1990 GHG emission inventory / 2020 emissions limit of 427 million metric tons of carbon dioxide equivalent (MMTCO<sub>2e</sub>) on December 6, 2007.

Analysis

## DISCUSSION

This project would result in the construction and operation of a church and rectory in Winters, California that would result in approximately 122 trips per day. Based on the size of the project and discussions with the YSAQMD, it is determined that it is unlikely that a project of this size would significantly contribute GHGs to California's emission

inventory. The YSAQMD does not require projects of this size to quantify their generation of GHGs (Jones, pers. comm.). Therefore, it is unlikely that the project would hinder or delay the State's ability to meet the reduction targets contained in AB 32. The URBEMIS2007 v9.2.4 (URBEMIS) model was used to determine the amount of trips the project will likely contribute to local roadways for the CO impact screening, as described in Question B above.

URBEMIS also contains a CO2 output that includes CO2 generation from the project's consumption of natural gas, hearth emissions, landscape equipment combustion, and mobile vehicle emissions. Although the URBEMIS model does not account for all GHG pollutants and emissions sources related to the project, it does include the main sources of CO2 from the development – on-road mobile vehicles. As such, it can be used as a rough tool to ascertain the project's relative contribution of GHGs. According to the modeling conducted, the project may contribute 192.3 tons of CO2 from operational activities in 2009, which is approximately 174.5 metric tons of CO2, or 0.0002 MMTCO2e. As such, the project may contribute 0.000041 percent of California's 2020 GHG emission limit of 427 MMTCO2e. Furthermore, the emissions estimate provided above is for the 2009 modeling year. With advancements in technology and implementation of existing and proposed state regulations, the project's emissions in 2020 will be lower than the 2009 estimates.

#### **SUMMARY**

The analysis supports the following conclusions:

- The project will not exceed the YSAQMD's regional and localized thresholds;
- The project will not result in a significant impact for the CEQA Guidelines Appendix G criteria; and
- The project will not significantly hinder or delay the State's ability to meet the emission reduction targets contained in AB 32.

| Issues   | Potentially Significant Impact | Potentially Significant Unless Mitigation Incorporated | Less-Than-Significant Impact        | No Impact                           |
|--|--------------------------------|--|-------------------------------------|-------------------------------------|
| <b>4. BIOLOGICAL RESOURCES.</b><br><i>Would the project:</i>   |                                |  |                                     |                                     |
| a. Have a substantial adverse effect, either directly or through habitat modifications, on any species identified as a candidate, sensitive, or special status species in local or regional plans, policies, or regulations, or by the California Department of Fish and Game or US Fish and Wildlife Service? | <input type="checkbox"/>       | <input checked="" type="checkbox"/>                    | <input type="checkbox"/>            | <input type="checkbox"/>            |
| b. Have a substantial adverse impact on any riparian habitat or other sensitive natural community identified in local or regional plans, policies, and regulations or by the California Department of Fish and Game or US Fish and Wildlife Service?   | <input type="checkbox"/>       | <input type="checkbox"/>                               | <input type="checkbox"/>            | <input checked="" type="checkbox"/> |
| c. Have a substantial adverse effect on federally protected wetlands as defined by Section 404 of the Clean Water Act (including, but not limited to, marsh, vernal pool, coastal, etc.) through direct removal, filling, hydrological interruption, or other means?   | <input type="checkbox"/>       | <input type="checkbox"/>                               | <input type="checkbox"/>            | <input checked="" type="checkbox"/> |
| d. Interfere substantially with the movement of any native resident or migratory fish or wildlife species or with established resident or migratory wildlife corridors, or impede the use of wildlife nursery sites?   | <input type="checkbox"/>       | <input checked="" type="checkbox"/>                    | <input type="checkbox"/>            | <input type="checkbox"/>            |
| e. Conflict with any local policies or ordinances protecting biological resources, such as a tree preservation policy or ordinance?  | <input type="checkbox"/>       | <input type="checkbox"/>                               | <input checked="" type="checkbox"/> | <input type="checkbox"/>            |
| f. Conflict with the provisions of an adopted Habitat Conservation Plan, Natural Conservation Community Plan, or other approved local, regional, or state habitat conservation plan?   | <input type="checkbox"/>       | <input checked="" type="checkbox"/>                    | <input type="checkbox"/>            | <input type="checkbox"/>            |

**Discussion**

a. & d. Approximately a third of the 5.56 acre site is developed with an existing community center, associated parking lots and commercial landscaping. The remainder of the site where the church, new parking and rectory are proposed to be located is vacant land with annual grassland vegetation that is disked and/or mowed periodically. The site is bounded by single family homes to the south, playfields of the elementary school to the east. The area proposed for the church and rectory is used for overflow parking on a regular basis for community center events. A number of relatively small trees are scattered along the southern and western property lines.

Wildlife use of the project area is limited as a result of the disturbed habitat conditions, small size, and the site's location within an urbanized area. The property lacks wetlands, vernal pools, and it has been the repeatedly disked and mowed.

The area also receives substantial human disturbance due to its close proximity to the community center, neighboring residential areas, and the elementary school.

The following Special-Status Wildlife potentially occur in the Winters area:

- Valley Elderberry Longhorn Beetle (VELB)
- Vernal Pool Fairy Shrimp
- Vernal Pool Tadpole Shrimp

The site does not contain blue elderberry (*Sambucus mexicana*) upon which VELB is dependent. Nor does it support vernal pool species. Therefore, VELB and vernal pool species will not be impacted by the proposed project.

- Northwestern Pond Turtle
- Tiger Salamander
- California Tiger Salamander
- California Red-legged Frog
- Giant Garter Snake

Pond turtles, giant garter snakes, and California red-legged frogs require slow-moving rivers, streams, or ponds with permanent or near permanent water sources. These habitats do not occur on the site, therefore, these species will not be impacted by the proposed project.

California tiger salamanders require seasonal wetland features such as vernal pools for egg laying and during their development stages. Due to the lack of wetlands on the property, this species will not be impacted by the proposed project.

- Bald Eagle
- Yellow Breasted Chat
- Western Burrowing Owl
- Swainson's Hawk
- White-tailed Kite

Bald eagles nest in mountainous areas near large, permanent water bodies such as lakes reservoirs, and river systems. The lack of suitable habitat on the site precludes the presence of this species. Therefore, this species will not be impacted by the proposed project.

Yellow-breasted chats inhabit riparian thickets. Given that there is no riparian vegetation on the site, this species will not be impacted by the proposed project.

Western burrowing owls require relatively open grassland habitat with suitable natural burrows or artificial burrows such as pipes, culverts, and debris piles that

can be used for nesting. While habitat conditions are considered marginal due to the extent of human disturbance, Burrowing Owl could potentially nest and forage in the project area. With the applicant's agreement to accept and implement the following mitigation measures, impacts on special status species would be less than significant.

***Mitigation Measure Biological 1*** – *The project proponent shall mitigate for potential project-related impacts to burrowing owl by conducting a pre-construction survey no more than 30 days prior to the initiation of construction activity. The pre-construction survey shall be conducted by a qualified biologist familiar with the identification of burrowing owls and the signs of burrowing owl activity. If active burrows are found on the project site, the California Department of Fish and Game (CDFG) shall be consulted regarding appropriate mitigation measures for project-related impacts to burrowing owl. Pursuant to the CDFG document entitled "Staff Report on Burrowing Owl Mitigation" (September 25, 1995), it is likely that replacement habitat will be required by CDFG. The guidelines include specific mitigation to protect nesting and wintering owls and to compensate for loss of breeding sites. In general, if the project would remove habitat of an occupied breeding site (e.g., if an active nest and surrounding habitat are removed), the project proponent will be required to compensate by preserving equivalent suitable habitat for each active nest site. In addition, the project proponent must install artificial burrows to offset the direct loss of the breeding site. Mitigation shall be consistent with the City's adopted Habitat Mitigation Program. Implementation of this mitigation measure shall be confirmed by the City of Winters prior to the initiation of construction activity.*

Swainson's Hawk could potentially forage in the grassland and seasonal wetland habitats, but no potential nesting trees are available onsite or in the immediate vicinity. Typically, CDFG considers annual grassland habitat within 10 miles of an active Swainson's Hawk nest to be potential foraging habitat for the species. However, the CDFG staff report regarding mitigation for impacts to Swainson's Hawk in the Central Valley acknowledges that projects that support less than 5 acres of foraging habitat and are surrounded by existing development do not provide the foraging habitat requirements needed to sustain the reproductive efforts of a Swainson's Hawk pair, unless there is a known nest within a ¼ mile of the project. Consequently, CDFG does not recommend that the CEQA lead agency require foraging mitigation for these types of projects.

Although the conditions on site and adjacent to the site provide limited nesting habitat for raptors, there is some potential for a raptor to nest on the site or within close proximity. With the applicant's agreement to accept and implement the following mitigation measures, impacts on special status raptors would be less than significant.

***Mitigation Measure Biological 2*** – *The project proponent shall mitigate for potential project-related impacts to nesting raptors (Swainson's Hawk, White-tailed Kite, Northern Harrier, and Loggerhead Shrike) by conducting a pre-construction survey of all trees suitable for use by nesting raptors on the subject property or within 0.25 mile of the project boundary as allowable. The preconstruction survey shall be performed no more than 30 days prior to the implementation of construction activities. The preconstruction survey shall be conducted by a qualified biologist familiar with the identification of raptors known to occur in the vicinity of the City of Winters. If active special-status raptor nests are found during the preconstruction survey, a 0.25-mile (1,320-feet) buffer zone shall be*

*established around the nest and no construction activity shall be conducted within this zone during the raptor nesting season. The buffer zone shall be marked with flagging, construction lathe, or other means to mark the boundary of the buffer zone. All construction personnel shall be notified as to the existence of the buffer zone and to avoid entering the buffer zone during the nesting season. Implementation of this mitigation measure shall be confirmed by the City of Winters prior to the initiation of construction activity.*

The trees and grassland on the site provide suitable nesting habitat for a number of common and special-status birds protected solely by the Migratory Bird Treaty Act (MBTA) which prohibits the killing of migratory birds. Therefore, if any vegetation or tree removal occurs during the typical avian nesting season (February 1 to August 31), a pre-construction survey is necessary. With the applicant's agreement to accept and implement the following mitigation measures, impacts on migratory birds would be less than significant.

***Mitigation Measure Biological 3*** – *The project proponent shall mitigate for potential project-related impacts to migratory birds by conducting a pre-construction survey for nests on the site. The preconstruction survey shall be performed no more than 14 days prior to the onset of vegetation and/or tree removal. The preconstruction survey shall be conducted by a qualified biologist familiar with the identification of migratory bird known to occur in the vicinity of the City of Winters. If active migratory bird nest(s) are found onsite during the preconstruction survey, the nest(s) shall not be disturbed or removed until the young have fledged and the nest is no longer active. A buffer may be required. All construction personnel shall be notified as to the existence of the buffer zone and to avoid entering the buffer zone during the nesting season. Implementation of this mitigation measure shall be confirmed by the City of Winters prior to the initiation of construction activity.*

*Alternatively, potential impacts to nesting birds or unfledged young would be avoided if vegetation and/or tree removal occurred only between September 1 and January 21.*

b. & c. Sensitive habitats include those that are of special concern to resource agencies or those that are protected under CEQA, Section 1600 of the California Fish and Game Code, or Section 404 of the Clean Water Act. There are no wetlands, riparian areas, or sensitive habitats located on or adjacent to the site. Therefore, the proposed project will not impact these resources.

e. The trees on the site are not listed on the City's Historic Tree list, the site does not contain wetlands, and with compliance with the above mitigation measures for burrowing owl, Swainson's Hawk, raptors, and migratory birds would be consistent with the natural resources polices contained in the City's General Plan. This impact is less than significant.

f. No Habitat Conservation Plan, Natural Community Conservation Plan, or other approved local, regional, or state habitat conservation plan has been adopted for the project site. The County and cities are in the process of

developing a countywide plan, but it is not complete. The City's Habitat Management Program requires "on the ground" mitigation to be located within 7 miles of Winters in order to provide locally beneficial mitigation. With compliance with the Winters Habitat Mitigation Program, this impact is less than significant.

**Mitigation Measure Biological 4** – Any mitigation required shall be implemented in a manner consistent with requirements, purpose and intent of the City of Winters' Habitat Mitigation Program.

| Issues   | Potentially Significant Impact | Potentially Significant Unless Mitigation Incorporated | Less-Than-Significant Impact | No Impact                |
|--|--------------------------------|--|------------------------------|--------------------------|
| <b>5. CULTURAL RESOURCES.</b><br><i>Would the project:</i>   |                                |  |                              |                          |
| a. Cause a substantial adverse change in the significance of a historical resource as defined in Section 15064.5?    | <input type="checkbox"/>       | <input checked="" type="checkbox"/>                    | <input type="checkbox"/>     | <input type="checkbox"/> |
| b. Cause a substantial adverse change in the significance of an archaeological resource pursuant to Section 15064.5? | <input type="checkbox"/>       | <input checked="" type="checkbox"/>                    | <input type="checkbox"/>     | <input type="checkbox"/> |
| c. Directly or indirectly destroy a unique paleontological resource or site, or unique geologic feature?             | <input type="checkbox"/>       | <input checked="" type="checkbox"/>                    | <input type="checkbox"/>     | <input type="checkbox"/> |
| d. Disturb any human remains, including those interred outside of formal cemeteries.                                 | <input type="checkbox"/>       | <input checked="" type="checkbox"/>                    | <input type="checkbox"/>     | <input type="checkbox"/> |

**Discussion**

a. & b. A cultural resources assessment was prepared for this site by Far Western Anthropological Research Associates (August, 2008). The assessment provides the results from the research of existing cultural resources data bases, review of historic maps, and a field survey performed by a qualified archeologist. The entire property was inspected. No evidence of cultural resources was observed.

Although no evidence of cultural resources was observed in the study area, there is always the possibility that unidentified resources could be encountered on or below the surface during grading and construction. With the applicant's agreement to accept and implement the following mitigation measure related to unknown sub-surface cultural resources, the potential for impact would be mitigated to a less than significant level by ensuring that such resources are evaluated and protected as appropriate.

**Mitigation Measure Cultural 1** – If cultural resources (historic, archeological, paleontological, and/or human remains) are encountered during construction, workers shall not alter the materials or their context until an appropriately trained cultural resource consultant has evaluated the situation. Project personnel shall not collect cultural

resources. Prehistoric resources include chert or obsidian flakes, projectile points, mortars, pestles, dark friable soil containing shell and bone dietary debris, heat-affected rock, or human burials. Historic resources include stone or adobe foundations or walls, structures and remains with square nails, and refuse deposits often in old wells and privies.

- c. No paleontological resources are known or suspected and no unique geologic features exist on the project site. However, the potential exists during construction to uncover previously unidentified resources. Implementation of Mitigation Measure Cultural 1 will mitigate this concern to less than significant levels.
- e. No human remains are known or predicted to exist in the project area. However, the potential exists during construction to uncover previously unidentified resources. Compliance with Section 7050.5 of the California Health and Safety Code will reduce the potential impact to a less than significant level.

**Mitigation Measure Cultural 2** - Should human remains be discovered, no further site disturbance shall occur until the county coroner has determined that the remains are not subject to the provisions of Section 27491 of the Government Code or any other related provisions of law concerning investigation of the circumstances, manner and cause of any death, and the recommendations concerning the treatment and disposition of the human remains have been made to the person responsible for the excavation, in the manner provided in Section 5097.98 of the Public Resources Code. If the coroner determines that the remains are not subject to his or her authority and the remains are recognized to be those of a Native American, the coroner shall contact the Native American Heritage Commission within 24 hours.

| Issues  | Potentially Significant Impact | Potentially Significant Unless Mitigation Incorporated | Less-Than-Significant Impact        | No Impact                           |
|---|--------------------------------|--|-------------------------------------|-------------------------------------|
| <b>6. GEOLOGY AND SOILS.</b>  |                                |  |                                     |                                     |
| <i>Would the project:</i>   |                                |  |                                     |                                     |
| a. Expose people or structures to potential substantial adverse effects, including the risk of loss, injury, or death involving:  |                                |  |                                     |                                     |
| i. Rupture of a known earthquake fault as delineated on the most recent Alquist - Priolo Earthquake Fault Zoning Map issued by the State Geologist for the area or based on other substantial evidence of a known fault? Refer to Division of Mines and Geology Special Publication 42. | <input type="checkbox"/>       | <input type="checkbox"/>                               | <input checked="" type="checkbox"/> | <input type="checkbox"/>            |
| ii. Strong seismic ground shaking?  | <input type="checkbox"/>       | <input type="checkbox"/>                               | <input checked="" type="checkbox"/> | <input type="checkbox"/>            |
| iii. Seismic-related ground failure, including liquefaction?  | <input type="checkbox"/>       | <input checked="" type="checkbox"/>                    | <input type="checkbox"/>            | <input type="checkbox"/>            |
| iv. Landslides?   | <input type="checkbox"/>       | <input type="checkbox"/>                               | <input type="checkbox"/>            | <input checked="" type="checkbox"/> |

- |   |                          |                                     |                          |                                     |
|---|--------------------------|-------------------------------------|--------------------------|-------------------------------------|
| b. Result in substantial soil erosion or the loss of topsoil?   | <input type="checkbox"/> | <input type="checkbox"/>            | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| c. Be located on a geologic unit or soil that is unstable, or that would become unstable as a result of the project, and potentially result in on-or off-site landslide, lateral spreading, subsidence, liquefaction or collapse? | <input type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/>            |
| d. Be located on expansive soils, as defined in Table 18-1-B of the Uniform Building Code (1994), creating substantial risks to life or property?   | <input type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/>            |
| e. Have soils incapable of adequately supporting the use of septic tanks or alternative wastewater disposal systems where sewers are not available for the disposal of wastewater?  | <input type="checkbox"/> | <input type="checkbox"/>            | <input type="checkbox"/> | <input checked="" type="checkbox"/> |

**Discussion**

The subject site is situated geologically in the Sacramento Valley, within the westerly portion of the Great Valley geomorphic province of California. Sands, silts, and clays encountered in the near vicinity are recognized as the upper member of the Quaternary-aged Modesto Formation. The soils of this unit are characterized as arkosic alluvium deposits.

According to the soil survey maps of the Natural Resources Conservation District (NRCS) (formerly the USDA Soil Conservation Service) the soil on the site is Brentwood silty clay loam, 0-2 percent slopes.

- ai, ii. There are no known faults within the City of Winters. The site is located approximately 6 km (3.7 miles) from the Great Valley Thrust Fault, as shown on recent maps by the U.S. Geological Survey and the California Geological Survey.

The Alquist-Priolo Special Studies Zones Act of 1972 regulates development near active faults to mitigate the hazard of surface fault rupture and prohibits the development of structures for human occupancy across the traces of active faults. The project site is not located within an Alquist-Priolo Special Studies Zone.

The City is located in an area of relatively low seismic activity. According to the Seismic Risk Map of the United States, Winters is in Zone 3. Within Zone 3, the potential for earthquakes is low; however, there is the possibility for major damage (VIII to X on the Modified Mercalli Scale from a nearby earthquake). A rating of VIII to X on the Modified Mercalli Scale generally means the Richter scale magnitude would be between 6.0 to 7.9. Effects associated with this intensity range from difficulty standing to broken tree branches to damage to foundations and frame structures to destruction of most masonry and frame structures.

Any major earthquake damage on the project site is likely to occur from ground shaking and seismically-related ground and structural failures. Local soil conditions, such as soil strength, thickness, density, water content, and firmness of underlying bedrock affect seismic response. Seismically-induced shaking and some damage should be expected to occur during an event, but damage should be no more severe in the project area than elsewhere in the region. Framed construction on proper foundations constructed in accordance with California Building Code requirements is generally flexible enough to sustain only minor structural damage from ground shaking. Therefore, people and structures would not be exposed to potential substantial adverse effects involving strong seismic ground shaking, and this would be a less than significant impact.

- a.iii, c.d. A Foundation Report was prepared for the previously proposed project by Laver L. Roper & Associates in 1988 and was updated by Raney Geotechnical in October 2008. The geologic investigation, which included 13 test borings throughout the project site in 1988 surface soil samples in 2008, found that surface and near-surface soils on the project site are capable of supporting public/quasi-public and residential structures of the type proposed for the project provided specified conditions are implemented. With the applicant's agreement to accept and implement the following mitigation measure, impacts of geologic hazards will be reduced to a less than significant level.

*Mitigation Measure Geology 1 – The applicant shall submit a soils and geotechnical report upon submittal of the initial improvement plans package. The improvement plans shall be signed by the soils engineer for conformance to the geotechnical report prior to approval by the City.*

- a.iv, b. The site topography is essentially flat with an elevation of 128 above mean sea level. Surface runoff flows toward either the existing storm water drain grates in the community center parking lots or to the surrounding streets. There are no steep slopes within the project site. There are no drainages with steep slopes running through or adjacent to the project site. Because the site conditions would not result in landslides or potential for substantial erosion or loss of topsoil, there would be no impact for this category.
- f. The project would construct sewer pipelines that connect to wastewater treatment facilities and would not involve the construction of septic tanks. Therefore, there would be no impact.

| Issues   | Potentially Significant Impact | Potentially Significant Unless Mitigation Incorporated | Less-Than-Significant Impact        | No Impact                           |
|--|--------------------------------|--|-------------------------------------|-------------------------------------|
| <b>7. HAZARDS AND HAZARDOUS MATERIALS.</b><br><i>Would the project</i>   |                                |  |                                     |                                     |
| a. Create a significant hazard to the public or the environment through the routine transport, use, or disposal of hazardous materials?  | <input type="checkbox"/>       | <input type="checkbox"/>                               | <input checked="" type="checkbox"/> | <input type="checkbox"/>            |
| b. Create a significant hazard to the public or the environment through reasonably foreseeable upset and accident conditions involving the release of hazardous materials into the environment?  | <input type="checkbox"/>       | <input type="checkbox"/>                               | <input checked="" type="checkbox"/> | <input type="checkbox"/>            |
| c. Emit hazardous emissions or handle hazardous or acutely hazardous materials, substances, or waste within one-quarter mile of an existing or proposed school?  | <input type="checkbox"/>       | <input type="checkbox"/>                               | <input checked="" type="checkbox"/> | <input type="checkbox"/>            |
| d. Be located on a site which is included on a list of hazardous materials sites compiled pursuant to Government Code Section 65962.5 and, as a result, would it create a significant hazard to the public or the environment?                                   | <input type="checkbox"/>       | <input type="checkbox"/>                               | <input type="checkbox"/>            | <input checked="" type="checkbox"/> |
| e. For a project located within an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project result in a safety hazard for people residing or working in the project area? | <input type="checkbox"/>       | <input type="checkbox"/>                               | <input type="checkbox"/>            | <input checked="" type="checkbox"/> |
| f. For a project within the vicinity of a private airstrip, would the project result in a safety hazard for people residing or working in the project area?  | <input type="checkbox"/>       | <input type="checkbox"/>                               | <input type="checkbox"/>            | <input checked="" type="checkbox"/> |
| g. Impair implementation of or physically interfere with an adopted emergency response plan or emergency evacuation plan?  | <input type="checkbox"/>       | <input type="checkbox"/>                               | <input type="checkbox"/>            | <input checked="" type="checkbox"/> |
| h. Expose people or structures to the risk of loss, injury or death involving wildland fires, including where wildlands are adjacent to urbanized areas or where residences are intermixed with wildlands?   | <input type="checkbox"/>       | <input type="checkbox"/>                               | <input type="checkbox"/>            | <input checked="" type="checkbox"/> |

**Discussion**

a, b, & c. During construction, oil, diesel fuel, gasoline, hydraulic fluid, and other liquid hazardous materials would be used at the project site. Similarly, paints, solvents, and various architectural finishes would be used during construction.

If spilled, these substances could pose a risk to the environment and to human health. In the event of a spill, the City of Winters Fire Department is responsible

for responding to non-emergency hazardous materials reports. The use, handling, and storage of hazardous materials are highly regulated by both the Federal Occupational Safety and Health Administration (Fed/OSHA) and the California Occupational Safety and Health Administration (Cal/OSHA). Cal/OSHA is responsible for developing and enforcing workplace safety regulations. Both federal and State laws include special provisions/training for safe methods for handling any type of hazardous substance. The City currently complies with the City's Emergency Response Plan, and the Yolo County Hazardous Waste Management Plan.

Because the types of uses associated with the proposed church and rectory do not typically use, transport or dispose of large amounts of hazardous materials, and the routine transport, use, and disposal of hazardous materials are regulated by federal, State, and local regulations, this impact is considered less than significant.

- d. The project is not located on a site that is included on a list of hazardous materials sites compiled by the CA Department of Toxic Substances Control EnviroStor Database list pursuant to Government Code 65962.5. Therefore, no impact would occur.
- e. The project site is not within two miles of a public airport, and is not within the runway clearance zones established to protect the adjoining land uses in the vicinity from noise and safety hazards associated with aviation accidents. Therefore, there would be no impact.
- f. There are no private airstrips in proximity of the project site, so there would be no impact.
- g,h. The proposed project would have no effect on any emergency plan, because it would not significantly alter the existing street system, and would provide street connections to and through the project site. The project area does not qualify as "wildlands" where wildland fires are a risk. For these reasons, no impact would occur in these categories.

| Issues  | Potentially Significant Impact | Potentially Significant Unless Mitigation Incorporated | Less-Than-Significant Impact        | No Impact                           |
|---|--------------------------------|--|-------------------------------------|-------------------------------------|
| <b>8. HYDROLOGY AND WATER QUALITY</b>   |                                |  |                                     |                                     |
| <i>Would the project:</i>   |                                |  |                                     |                                     |
| a. Violate any water quality standards or waste discharge requirements?   | <input type="checkbox"/>       | <input type="checkbox"/>                               | <input checked="" type="checkbox"/> | <input type="checkbox"/>            |
| b. Substantially deplete groundwater supplies or interfere substantially with groundwater recharge such that there would be a net deficit in aquifer volume or a lowering of the local groundwater table level (e.g., the production rate of pre-existing nearby wells would drop to a level which would not support existing land uses or planned uses for which permits have been granted)? | <input type="checkbox"/>       | <input type="checkbox"/>                               | <input checked="" type="checkbox"/> | <input type="checkbox"/>            |
| c. Substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river, in a manner which would result in substantial erosion or siltation on- or off-site?  | <input type="checkbox"/>       | <input type="checkbox"/>                               | <input checked="" type="checkbox"/> | <input type="checkbox"/>            |
| d. Substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river, or substantially increase the rate or amount of surface runoff in a manner which would result in flooding on- or off-site?   | <input type="checkbox"/>       | <input type="checkbox"/>                               | <input checked="" type="checkbox"/> | <input type="checkbox"/>            |
| e. Create or contribute runoff water which would exceed the capacity of existing or planned stormwater drainage systems to control?   | <input type="checkbox"/>       | <input type="checkbox"/>                               | <input checked="" type="checkbox"/> | <input type="checkbox"/>            |
| f. Otherwise substantially degrade water quality?   | <input type="checkbox"/>       | <input type="checkbox"/>                               | <input checked="" type="checkbox"/> | <input type="checkbox"/>            |
| g. Place housing within a 100-year floodplain, as mapped on a federal Flood Hazard Boundary or Flood Insurance Rate Map or other flood hazard delineation map?  | <input type="checkbox"/>       | <input type="checkbox"/>                               | <input checked="" type="checkbox"/> | <input type="checkbox"/>            |
| h. Place within a 100-year floodplain structures which would impede or redirect flood flows?  | <input type="checkbox"/>       | <input type="checkbox"/>                               | <input checked="" type="checkbox"/> | <input type="checkbox"/>            |
| i. Expose people or structures to a significant risk of loss, injury or death involving flooding, including flooding as a result of the failure of a levee or dam?  | <input type="checkbox"/>       | <input type="checkbox"/>                               | <input checked="" type="checkbox"/> | <input type="checkbox"/>            |
| j. Inundation by seiche, tsunami, or mudflow?   | <input type="checkbox"/>       | <input type="checkbox"/>                               | <input type="checkbox"/>            | <input checked="" type="checkbox"/> |

### Discussion

- a,f. Surface water quality can be adversely affected by erosion during project construction, or after the project is completed, if urban contaminants in storm water runoff are allowed to reach a receiving water (e.g. Putah Creek). Construction activities disturbing one or more acres are required by the Central Valley Regional Water Quality Control Board (CVRWQCB) to obtain a General

**Construction Activity Stormwater Permit and a National Discharge Elimination System (NPDES) permit. These permits are required to control both construction and operation activities that could adversely affect water quality. Permit applicants are required to prepare and retain at the construction site a Stormwater Pollution Prevention Plan (SWPPP) that describes the site, erosion and sediment controls, means of waste disposal, implementation of approved local plans, control of post-construction sediment and erosion control measures and maintenance responsibilities, and non-stormwater management controls. Dischargers are also required to inspect construction sites before and after storms to identify stormwater discharge from construction activity, and to identify and implement controls where necessary.**

**The proposed project is composed of approximately 5.56 acres, and thus would fall subject to these requirements. Compliance with these required permits would ensure that runoff during construction and occupation of the project site would ensure that runoff does not substantially degrade water quality. Therefore, this is a less than significant impact.**

- b. The proposed project would construct impervious surfaces over portions of the project site that are currently undeveloped. However, the site is not identified as a recharge area and has been planned for development since at least the late 1980s. The majority of groundwater recharge in Winters occurs along drainages. Therefore, it can be concluded that development of the project site would not substantially affect the aquifer.**

**The City of Winters would supply groundwater to the proposed project. As discussed in more detail in Item 16(d), while the proposed project would contribute to an increase in municipal groundwater use, total groundwater use within the City would exceed historic water use levels only slightly in wet years, and would be lower than historic pumping levels in wet years. Groundwater levels have been fairly stable in the City of Winters, even with the highest historic pumping levels. Therefore, impacts on groundwater would be less than significant.**

- c,d,e. The proposed project would nominally change absorption rates, drainage patterns, and the rate and amount of surface runoff, but would not alter the course of a river or stream. The City's storm drainage system has been planned to accommodate development of the General Plan, including the project site. Because the proposed project can be accommodated within the City's planned storm drain system, the increase in runoff is considered less than significant.**
- g,h. The project does not fall within the City's General Plan Flood Overlay Area. The site is designated on federal floodplain maps as Zone X (outside of the 100-year floodplain). As such impacts related to flooding are considered less than significant.**

- i. The project site is located approximately 10 miles east of the Monticello Dam on Lake Berryessa. Failure or overtopping of the dam could result in severe flooding of the Winters' area and loss of life. However, this occurrence, which is addressed in the Yolo County Emergency Plan, is not considered a likely or substantial risk. Therefore, the proposed project would not expose individuals to a substantial risk from flooding as a result of the failure, and the impact would be less than significant.
- j. The project area is not located near any large bodies of water that would pose a seiche or tsunami hazard. In addition, the project site is relatively flat and is not located near any physical or geologic features that would produce a mudflow hazard. Therefore, no impact would occur.

| Issues  | Potentially Significant Impact | Potentially Significant Unless Mitigation Incorporated | Less-Than-Significant Impact | No Impact                           |
|---|--------------------------------|--|------------------------------|-------------------------------------|
| <b>9. LAND USE AND PLANNING.</b>  |                                |  |                              |                                     |
| <i>Would the project:</i>   |                                |  |                              |                                     |
| a. Physically divide an established community?  | <input type="checkbox"/>       | <input type="checkbox"/>                               | <input type="checkbox"/>     | <input checked="" type="checkbox"/> |
| b. Conflict with any applicable land use plans, policies, or regulations of an agency with jurisdiction over the project (including, but not limited to the general plan, specific plan, local coastal program, or zoning ordinance) adopted for the purpose of avoiding or mitigating on environmental effect? | <input type="checkbox"/>       | <input checked="" type="checkbox"/>                    | <input type="checkbox"/>     | <input type="checkbox"/>            |
| c. Conflict with any applicable habitat conservation plan or natural communities conservation plan?   | <input type="checkbox"/>       | <input checked="" type="checkbox"/>                    | <input type="checkbox"/>     | <input type="checkbox"/>            |

**Discussion**

- a. Development of the project site is consistent with the City General Plan and has been the long-term plan for the property. The project would fill in and connect the established residential community of the City, not divide it. Therefore, no impact would occur.
- b. The General Plan and zoning ordinance currently designates the project site for medium density residential uses which includes quasi-public uses such as churches.

The applicant has applied for Design Review approval which includes an analysis of compliance with lot development standards, and a review of building and

landscape design, facades, and elevations to ensure that the proposed project will be compatible with existing development in Winters and that it satisfies the Community Design Guidelines.

The proposed church exceeds the R2 30 foot height limit. The building is single story and is located in the west central portion of the parcel. Due to the roofline, the building is 41' 4" tall with total height of 51' including the roof mounted cross.

The total onsite parking required for the site developed with the existing community center, existing auxiliary building, proposed church and proposed rectory would be 271 spaces per the parking standards in Section 17.72.020 of the City of Winters Municipal Code. The proposed project provides 236 spaces which amounts to 87% of the required spaces.

The applicant has requested a Planned Development Overlay as part of the project entitlements to modify the height restrictions and parking requirements for this property. Approval of the Planned Development Overlay will be necessary to accommodate the proposed project design.

With the applicant's agreement to accept and implement the following mitigation measures, this potential impact would be mitigated to a less than significant level.

*Mitigation Measure Land Use 1 - All aspects of the project shall be subject to design review to ensure compatibility with the surrounding area and satisfaction of the Community Design Guidelines and other applicable principles of good community design.*

*Mitigation Measure Land Use 2 - The proposed project height and parking provisions are subject to approval of a Planned Development Overlay for the subject property.*

- c. No Habitat Conservation Plan, Natural Community Conservation Plan, or other approved local, regional, or state habitat conservation plan has been adopted for the project site. The County and cities are in the process of developing a countywide plan, but it is not complete. The City's Habitat Management Program requires "on the ground" mitigation to be located within 7 miles of Winters in order to provide locally beneficial mitigation. With compliance with the Winters Habitat Mitigation Program per Mitigation Measure Biological 4 this impact is less than significant.

| Issues  | Potentially Significant Impact | Potentially Significant Unless Mitigation Incorporated | Less-Than-Significant Impact        | No Impact                |
|---|--------------------------------|--|-------------------------------------|--------------------------|
| <b>10. MINERAL RESOURCES.</b><br><i>Would the project:</i>  |                                |  |                                     |                          |
| a. Result in the loss of availability of a known mineral resource that would be of value to the region and the residents of the State?                                | <input type="checkbox"/>       | <input type="checkbox"/>                               | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| b. Result in the loss of availability of a locally important mineral resource recovery site delineated on a local general plan, specific plan or other land use plan? | <input type="checkbox"/>       | <input type="checkbox"/>                               | <input checked="" type="checkbox"/> | <input type="checkbox"/> |

**Discussion**

a,b. The project site is not designated as a mineral resource zone or locally important mineral resource recovery site. The construction of the proposed project would not result in the loss of any known mineral resources. Impacts would be less than significant.

| Issues  | Potentially Significant Impact | Potentially Significant Unless Mitigation Incorporated | Less-Than-Significant Impact        | No Impact                           |
|---|--------------------------------|--|-------------------------------------|-------------------------------------|
| <b>11. NOISE.</b><br><i>Would the project result in:</i>  |                                |  |                                     |                                     |
| a. Exposure of persons to or generation of noise levels in excess of standards established in the local general plan or noise ordinance, or applicable standards of other agencies?   | <input type="checkbox"/>       | <input type="checkbox"/>                               | <input checked="" type="checkbox"/> | <input type="checkbox"/>            |
| b. Exposure of persons to or generation of excessive groundborne vibration or groundborne noise levels?   | <input type="checkbox"/>       | <input type="checkbox"/>                               | <input checked="" type="checkbox"/> | <input type="checkbox"/>            |
| c. A substantial permanent increase in ambient noise levels in the project vicinity above levels existing without the project?  | <input type="checkbox"/>       | <input type="checkbox"/>                               | <input checked="" type="checkbox"/> | <input type="checkbox"/>            |
| d. A substantial temporary or periodic increase in ambient noise levels in the project vicinity above levels existing without the project?  | <input type="checkbox"/>       | <input checked="" type="checkbox"/>                    | <input type="checkbox"/>            | <input type="checkbox"/>            |
| e. For a project located within an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project expose people residing or working in the project area to excessive noise levels? | <input type="checkbox"/>       | <input type="checkbox"/>                               | <input type="checkbox"/>            | <input checked="" type="checkbox"/> |

- f. For a project within the vicinity of a private airstrip, would the project expose people residing or working in the project area to excessive noise levels?

### **Discussion**

Michael Brandman Associates prepared a noise analysis for the proposed project which included discussion of existing noise levels, sensitive receptors, and predicted noise level for both short-term and long-term activities associated with the project.

- a. Noise levels in the project area would be influenced by construction activity in the short term and by traffic, church activity (including church bells), parking lot, and HVAC noise in the long term.

#### **Short-term Noise**

Construction noise represents a short-term increase in ambient noise levels. Noise impacts from construction activities associated with the project would be a function of the noise generated by construction equipment, equipment location, the sensitivity of nearby land uses, and the timing and duration of the construction activities. Title 8, Health and Safety, of the Winters Municipal Code (WMC), Chapter 8.20 Noise Control, includes exterior noise levels limits similar to those established in the City of Winters General Plan Health and Safety Section (see Long-term Noise below). However, WMC Chapter 8.20.120 Exemptions, states that construction activity is exempt from the noise standards (provided it takes place on weekdays between 7:00 am and 7:00 pm per WMC Chapter 8.20.100 Prohibited Acts). Therefore, although there are no construction standards for noise generation, all construction activity is required to be conducted in accordance with the WMC, and therefore construction noise would not expose persons to or generate noise levels in excess of standards.

#### **Long-term Noise**

Traffic, church activity (including church bells), parking lot and HVAC noise all represent long-term sources of ambient noise in the project area. The City of Winters Land Use Compatibility Standards in Table II-2 of the General Plan Health and Safety Section establish the acceptable range of ambient noise levels for residential and schools within the City of Winters. Noise levels are normally acceptable at 60 Ldn dBA for residential uses, and 65 Ldn dBA for public buildings, including schools and churches. Noise levels are conditionally acceptable at 65 and 70 Ldn dBA for residences and public buildings, respectively.

Based on the project trip generation as discussed in the Air Quality analysis, increases in traffic from both the church and the rectory would result in an imperceptible increase in noise levels, less than 1 dB. Therefore, noise levels

would remain at 66 dB or less, as measured in April 2007. Although these noise levels are greater than the conditionally acceptable standard for residences, the project would not contribute to increased noise levels, and would not directly cause an exceedance of the standards. Other long-term noise as a result of the project, including church activities, parking lot noise, and HVAC operation, would also not result in exceedances of the standards. WMC Chapter 8.20.100 includes provisions for permitting the sounding of any electronically amplified signal from any stationary bell or chime, but indicates that houses of religious worship are exempt from this provision.

On the basis of short-term and long-term noise level estimates, the project would not result in noise levels that exceed City of Winters noise standards. Therefore, impacts would be less than significant.

- b. Construction activities can produce vibration that may be felt by adjacent uses. The construction of the proposed project would not require the use of equipment such as jackhammers and pile drivers, which are known to generate substantial construction vibration levels. The primary sources of vibration during construction would be from bulldozers, backhoes, crawler tractors, and scrapers. A vibratory roller would produce the greatest amount of vibration on the project site, with a 0.210 PPV at 25 feet. The nearest construction activities would be approximately 100 feet from the residences to the north of the project site, across West Grant Avenue, 175 feet from the residences to the south of the project site, and 350 feet from the school to the east of the project site. Construction vibration levels are expected to be 0.05 PPV, 0.03 PPV, and 0.015 PPV at the three nearest receptors, respectively. The maximum vibration the nearest residential receptor would be expected to experience is 0.05 PPV, which is below the 0.5 PPV significance level for potential structural damage. Therefore, construction-related vibration impacts from the project on existing sensitive receptors would be less than significant.

#### Operational Vibration

The project consists of the expansion of an existing church site, and it would not be expected to result in increased vibration in the project area, except for an occasional delivery truck, which would be expected for any public building. Therefore, operational vibration impacts from the project on existing sensitive receptors would be less than significant.

- c. The ongoing operation of the project would be affected by long-term ambient noise sources such as traffic (mobile), and church activity, parking lot and HVAC noise (stationary). Existing noise sources in the project area, which are limited to traffic, would not impact new receptors at the church.

### Mobile Noise

Long-term noise impacts would result from vehicle traffic associated with the project. The City of Winters Land Use Compatibility Standards in Table II-2 of the General Plan Health and Safety Section establish the acceptable range of ambient noise levels for residential and schools within the City of Winters. Noise levels are normally acceptable at 60 Ldn dBA for residential uses, and 65 Ldn dBA for public buildings, including schools and churches. Noise levels are conditionally acceptable at 65 and 70 Ldn dBA for residences and public buildings, respectively.

A traffic study was not prepared for this project, and therefore traffic volumes were not available for traffic noise prediction. Based on the project trip generation as discussed in the Air Quality analysis, increases in traffic from both the church and the Rectory would result in an imperceptible increase in noise levels, less than 1 dB. Therefore, noise levels would remain at 66 dB or less, as measured in April 2007. Although these noise levels are greater than the conditionally acceptable standard for residences, the project would not contribute to increased noise levels, and therefore not result in a substantial noise increase. Noise levels from increased traffic as a result of the project would not result in significant impacts.

### Stationary Noise

The principal onsite stationary noise impacts would occur as a result of the activities within the church and rectory buildings (including church bells), parking lot noise, and the operation of HVAC equipment. Church activity would increase over time as the new church would be built with a 700-seat capacity to accommodate future growth. Music and singing would be the sources of noise from church activity, and would not be expected to reach 65 dBA at nearby receptors. In addition, music and singing are not typically perceived as unwanted noise. Church bells would ring as follows: 5 to 10 times before each weekend mass (Saturday at 5:00 pm and Sunday at 10:45 am, 12:30 pm and 5:00 pm). Church bell noise is exempt from City of Winters noise standards, and like music and signing, is not typically perceived as unwanted noise. Parking lot noise, consisting of car doors slamming, and garden/landscape maintenance activities are considered intermittent noise events and would not represent a substantial contribution to the ambient noise levels. Rooftop mechanical equipment on the church and rectory buildings could be as close as 50 feet from the nearest residences. Predicted noise from HVAC equipment would be less than 60 dBA, based on information from other similar projects, and therefore would not expose nearby residents or receptors at the church to noise levels exceeding noise standards for both types of receptors. It is not expected that activities would take place at the church and adjacent community center simultaneously, however if this would occur, the nature of the activities would not be expected to result in significant impacts. The noise standards for the community center would be the

same as those for the church, and both facilities would be required to adhere to the noise limits. Impacts would be less than significant.

- d. **Construction noise represents a short-term increase in ambient noise levels. Noise impacts from construction activities associated with the project would be a function of the noise generated by construction equipment, equipment location, the sensitivity of nearby land uses, and the timing and duration of the construction activities. Short-term noise impacts could occur during construction activities either from the noise impacts created from the transport of workers and movement of construction materials to and from the project site, or from the noise generated onsite during demolition, ground clearing, excavation, grading, and construction activities. The worst-case construction noise levels for the project would be 90 dBA at 50 feet. Construction activities are carried out in discrete steps, each of which has a unique mix of equipment and, consequently, unique noise characteristics. These sequential phases would change the character of the noise levels surrounding the construction site as work progresses. Despite the variety in the type and size of construction equipment, similarities in the dominant noise sources and patterns of operation allow noise ranges to be categorized by work phase.**

**On the basis of their proximity to the project site, the residential land uses adjacent the project site are the sensitive receptors of most concern as they relate to construction noise. Estimated noise levels are 84, 80, and 74 dBA at the three nearest receptors, respectively. It should be noted that construction noise often varies significantly on a day-to-day basis, and the noise levels predicted represent a worst-case scenario. Although construction noise would last the duration of construction, it would be the most noticeable during building construction, and because the site is developed, grading activity would be minimized. These estimated noise levels represent a potentially significant impact.**

**In order to minimize disruption to existing residents, all construction activity would be performed in accordance with the Winters Municipal Code Noise requirements. Additionally, mitigation is proposed below that would require noise attenuation measures incorporated into the project. Noise levels from construction after the application of mitigation measures that can be quantified, including distance requirements for construction activity and staging, and the use of portable acoustic barriers, would be approximately 78, 74, and 68 dBA at the three nearest receptors, respectively. The actual levels would be expected to be lower based on attenuation measures that cannot be quantified. Although there are no noise standards for construction activity, and construction activity is exempt from noise standards, the implementation of the noise attenuation measures in the mitigation below would result in noise levels still greater than the residential standards, but not at the school. However, impacts would be**

considered less than significant following the implementation of mitigation.

**Mitigation Measures Noise 1-** *The project applicant shall submit a construction noise mitigation plan to the City of Winters for review and approval. The plan shall depict the location of construction equipment and describe how noise would be mitigated through methods such as, but not limited to, locating stationary noise-generating equipment (such as pumps and generators) as far as possible from nearby noise-sensitive receptors. Where practicable, noise-generating equipment will be shielded from nearby noise-sensitive receptors by noise-attenuating buffers such as structures or haul trucks. Onsite noise sources such as heavy equipment located less than 200 feet from noise-sensitive receptors will be equipped with noise-reducing engine housings. Portable acoustic barriers able to attenuate at least 6 dB will be placed around noise-generating equipment located within 200 feet of existing residences. Water tanks and equipment storage, staging, and warm-up areas shall be located as far from noise-sensitive receptors as possible. All noise attenuation measures identified in the plan shall be incorporated into the project.*

**Mitigation Measure Noise 2 -** *Construction activities shall adhere to the following noise requirements:*

*All construction equipment shall utilize noise reduction features (e.g., mufflers and engine shrouds) that are no less effective than those originally installed by the manufacturer.*

*Hours of construction shall comply with those established in Chapter 8.20.100 of the Winters Municipal Code. Those hours are weekdays from 7:00 a.m. through 7:00 p.m. Construction is prohibited on weekends and federal holidays.*

- e. The nearest public airport is over 12 miles away and the project site is not within an airport land use plan. Therefore, project residents would not be exposed to excessive air traffic noise, thus there would be no impact.
- f. The project site is not located near a private airstrip and would not be exposed to noise from the private airstrip, so no impact would occur.

| Issues   | Potentially Significant Impact | Potentially Significant Unless Mitigation Incorporated | Less-Than-Significant Impact        | No Impact                           |
|--|--------------------------------|--|-------------------------------------|-------------------------------------|
| <b>12. POPULATION AND HOUSING.</b><br><i>Would the project:</i>  |                                |  |                                     |                                     |
| a. Induce substantial growth in an area, either directly (for example, by proposing new homes and businesses) or indirectly (for example, through extension of roads or other infrastructure)? | <input type="checkbox"/>       | <input type="checkbox"/>                               | <input checked="" type="checkbox"/> | <input type="checkbox"/>            |
| b. Displace substantial numbers of existing housing, necessitating the construction of replacement housing elsewhere?  | <input type="checkbox"/>       | <input type="checkbox"/>                               | <input type="checkbox"/>            | <input checked="" type="checkbox"/> |
| c. Displace substantial numbers of people, necessitating the construction of replacement housing elsewhere?  | <input type="checkbox"/>       | <input type="checkbox"/>                               | <input type="checkbox"/>            | <input checked="" type="checkbox"/> |

**Discussion**

- a. The proposed project is generally consistent with the 1992 General Plan assumptions for the area. Religious facilities are not growth inducing and no additional infrastructure is required which could induce growth. Therefore, infrastructure, services, and utilities are master planned to accommodate the proposed level of growth. The proposed project is infill in an urbanized area and does not require the extension of roads and other infrastructure to the project site. Because the development of the project site is consistent with the planning assumptions of the General Plan, the proposed project would not induce growth.
- b,c. A portion of the project site is developed with an existing community center and associated landscaping and parking. The remainder of the site is an undeveloped field used for community center overflow parking. The project involves no displacement of housing or people. Thus, there would be no impact.

| Issues   | Potentially Significant Impact | Potentially Significant Unless Mitigation Incorporated | Less-Than-Significant Impact        | No Impact                |
|--|--------------------------------|--|-------------------------------------|--------------------------|
| <b>13. PUBLIC SERVICES.</b><br><i>Would the project result in substantial adverse physical impacts associated with the provision of new or physically altered governmental facilities, need for new or physically altered governmental facilities, the construction of which could cause significant environmental impacts, in order to maintain acceptable service ratios, response times or other performance objectives for any of the public services:</i> |                                |  |                                     |                          |
| a. Fire protection?  | <input type="checkbox"/>       | <input type="checkbox"/>                               | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| b. Police protection?  | <input type="checkbox"/>       | <input type="checkbox"/>                               | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| c. Schools?  | <input type="checkbox"/>       | <input type="checkbox"/>                               | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| d. Parks?  | <input type="checkbox"/>       | <input type="checkbox"/>                               | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| e. Other public facilities?  | <input type="checkbox"/>       | <input type="checkbox"/>                               | <input checked="" type="checkbox"/> | <input type="checkbox"/> |

**Discussion**

a. & b. The Winters Fire Protection District provides primary fire protection service to the project site. The City of Winters Police Department provides primary police protection service. The proposed project could nominally increase demand for these fire and police protection services by increasing the amount of development and businesses within the Departments' service areas. This increase in development is consistent with City plans for the project site, as reflected in the General Plan. Because the project site is already in the City, the proposed project would not increase the size of the service area of the Fire District or Police Department. Thus, the proposed project would have a less than significant adverse impact.

c, d, & e. The proposed project is for a church and a rectory. As such, it does not require trigger requirements for school services, parkland, or other public facilities.

| Issues   | Potentially Significant Impact | Potentially Significant Unless Mitigation Incorporated | Less-Than-Significant Impact        | No Impact                |
|--|--------------------------------|--|-------------------------------------|--------------------------|
| <b>14. RECREATION.</b>   |                                |  |                                     |                          |
| a. Would the project increase the use of existing neighborhood and regional parks or other recreational facilities such that substantial physical deterioration of the facility would occur or be accelerated? | <input type="checkbox"/>       | <input type="checkbox"/>                               | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| b. Does the project include recreational facilities or require the construction or expansion of recreational facilities which might have an adverse physical effect on the environment?                        | <input type="checkbox"/>       | <input type="checkbox"/>                               | <input checked="" type="checkbox"/> | <input type="checkbox"/> |

**Discussion**

a & b. As discussed in Item 13(c,d and e), the proposed church and rectory would not generate recreational demands. This is a less than significant impact.

| Issues   | Potentially Significant Impact | Potentially Significant Unless Mitigation Incorporated | Less-Than-Significant Impact        | No Impact                           |
|--|--------------------------------|--|-------------------------------------|-------------------------------------|
| <b>15. TRANSPORTATION/CIRCULATION.</b>   |                                |  |                                     |                                     |
| <i>Would the project:</i>  |                                |  |                                     |                                     |
| a. Cause an increase in traffic which is substantial in relation to the existing load and capacity of the street system (i.e., result in a substantial increase in either the number of vehicle trips, the volume to capacity ratio on roads, or congestion at intersections)? | <input type="checkbox"/>       | <input type="checkbox"/>                               | <input checked="" type="checkbox"/> | <input type="checkbox"/>            |
| b. Exceed, either individually or cumulatively, a level of service standard established by the county congestion management agency for designated roads or highways?   | <input type="checkbox"/>       | <input type="checkbox"/>                               | <input checked="" type="checkbox"/> | <input type="checkbox"/>            |
| c. Result in a change in air traffic patterns, including either an increase in traffic levels or a change in location that results in substantial safety risks?  | <input type="checkbox"/>       | <input type="checkbox"/>                               | <input type="checkbox"/>            | <input checked="" type="checkbox"/> |
| d. Substantially increase hazards due to a design feature (e.g., sharp curves or dangerous intersections) or incompatible uses (e.g., farm equipment)?   | <input type="checkbox"/>       | <input type="checkbox"/>                               | <input type="checkbox"/>            | <input checked="" type="checkbox"/> |

- e. Result in inadequate emergency access?
- f. Result in inadequate parking capacity?
- g. Conflict with adopted policies supporting alternative transportation (e.g., bus turnouts, bicycle racks)?

**Discussion**

The Winters Highlands, Callahan Estates, Ogando-Hudson & Creekside Estates Traffic Impact Study (July, 2004) was prepared to examine the impacts from proposed development projects in the City of Winters which would impact the area around the proposed church and rectory. The analysis provides information on the potential effects associated with increases in traffics volumes at eight local intersections as a result of anticipated development in Winters. Based on the findings of the Study, the following level of service (LOS) and traffic volumes were identified in the vicinity of the proposed church and rectory:

**Level of Service**

| Intersection                  | AM Peak Hour |           | PM Peak Hour |           |
|-------------------------------|--------------|-----------|--------------|-----------|
|                               | Existing     | Projected | Existing     | Projected |
| Grant Avenue/Railroad Avenue  | C            | D         | C            | C         |
| Grant Avenue/Hemenway Street  | B            | C         | C            | B         |
| Grant Avenue/West Main Street | B            | C         | C            | C         |
| Grant Avenue/Valley Oak Drive | A            | B         | B            | B         |

**Traffic Volumes**

| Roadway Segment                                  | Existing                     |                 | Projected                    |                 |
|--|------------------------------|-----------------|------------------------------|-----------------|
|  | General Plan Daily Threshold | Existing Volume | General Plan Daily Threshold | Forecast Volume |
| Grant Avenue: Railroad Avenue to W. Main Street  | 15,000                       | 8,100           | 15,000                       | 9,700           |
| Grant Avenue: W. Main Street to Valley Oak Drive | 15,000                       | 4,500           | 30,000                       | 6,300           |

The City of Winters' General Plan contains the following Policies regarding Level of Service:

Policy III.A.1 – The City shall endeavor to maintain a Level of Service “C” or better as defined by the 1985 Highway Capacity Manual or subsequent revision, on all streets and intersections within the City.

**Policy III.A.8 – The City shall comply with and implement that program and policies of the Yolo County Congestion Management Plan (CMP).**

**The CMP identifies LOS D as the threshold for Grant Avenue and Railroad Avenue.**

- a. & b. Existing and projected traffic volumes and levels of service are consistent with the City's General Plan Policies. Based upon the Institute of Transportation Engineers' (ITE) trip generation rates for 'Place of Worship' and 'Single-Family Housing', the project would only contribute approximately 112 trips per day for the church expansion and 10 trips per day for the Rectory. This low level of trip generation is not likely to impact the LOS of nearby intersections and would be a less than significant impact.**
- c. The project site is not located near an airport and it does not include any improvements to airports or change in air traffic patterns. No impact would occur.**
- d,e. The proposed project includes land uses that are similar to other development in the project vicinity. The circulation system does not include any tight curves or other design hazards. As discussed in Item 15a,b above, the minor amount of average daily trips would not substantially increase congestion on local roadways given the existing and projected traffic levels. For these reasons, there would be no adverse impacts related to roadway hazards or interference with emergency access.**
- f. The existing community center, auxiliary building and proposed church and rectory would require a total of 271 parking spaces. There are currently 78 parking spaces on-site. The site plan provides for an additional 158 on-site spaces for a total of 236 spaces including 9 ADA spaces. Based upon the parking ratios listed in the City of Winters Municipal Code (17.72.020), the existing community center, auxiliary building and proposed church and rectory would require at total of 271 parking spaces. The 236 spaces proposed are approximately 87% of the required parking.**

**Currently, overflow parking on neighborhood streets and adjacent vacant lots occurs when large events are held at the community center. Development of the church and rectory will convert some of the ad hoc parking area. The development of additional on-site parking will be beneficial but overflow neighborhood parking is would continue to be expected during large events. This represents a status quo and as such is a less than significant impact.**

- g. The project would not conflict with adopted policies, plans, or programs supporting alternative transportation. The project includes appropriate pedestrian and bicycle route connections. Therefore, this impact would be less than significant.**

| Issues  | Potentially Significant Impact | Potentially Significant Unless Mitigation Incorporated | Less-Than-Significant Impact        | No Impact                |
|---|--------------------------------|--|-------------------------------------|--------------------------|
| <b>16. UTILITIES AND SERVICE SYSTEMS.</b>   |                                |  |                                     |                          |
| <i>Would the project:</i>   |                                |  |                                     |                          |
| a. Exceed wastewater treatment requirements of the applicable Regional Water Quality Control Board?   | <input type="checkbox"/>       | <input type="checkbox"/>                               | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| b. Require or result in the construction of new water or wastewater treatment facilities or expansion of existing facilities, the construction of which could cause significant environmental effects?                            | <input type="checkbox"/>       | <input checked="" type="checkbox"/>                    | <input type="checkbox"/>            | <input type="checkbox"/> |
| c. Require or result in the construction of new storm water drainage facilities or expansion of existing facilities, the construction of which could cause significant environmental effects?                                     | <input type="checkbox"/>       | <input type="checkbox"/>                               | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| d. Have sufficient water supplies available to serve the project from existing entitlements and resources, or are new or expanded entitlements needed?  | <input type="checkbox"/>       | <input checked="" type="checkbox"/>                    | <input type="checkbox"/>            | <input type="checkbox"/> |
| e. Result in a determination by the wastewater treatment provider which serves or may serve the project that it has adequate capacity to serve the project's projected demand in addition to the provider's existing commitments? | <input type="checkbox"/>       | <input checked="" type="checkbox"/>                    | <input type="checkbox"/>            | <input type="checkbox"/> |
| f. Be served by a landfill with sufficient permitted capacity to accommodate the project's solid waste disposal needs?  | <input type="checkbox"/>       | <input type="checkbox"/>                               | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| g. Comply with federal, state, and local statutes and regulations related to solid waste?   | <input type="checkbox"/>       | <input type="checkbox"/>                               | <input checked="" type="checkbox"/> | <input type="checkbox"/> |

**Discussion**

- a. Public sewer service is available adjacent to the project site. The proposed church and rectory will be required to connect to City sewage treatment plant for wastewater treatment. The City's plant is permitted by the State and must meet applicable water quality standards. As a public/quasi-public and residential development, the proposed project is not anticipated to generate wastewater that contains unusual types or levels of contaminants, so it would not inhibit the ability of the Winters Wastewater Treatment Plant (WWTP) to meet State water quality standards. For these reasons, this would be a less than significant impact.
  
- b,e. Municipal water is proposed to be provided to the site via the existing 12 inch water main on the north side of the property and the 10 inch water main on the west side of the property. Water would be conveyed within the site via an 8 and

4 inch lines which are proposed to connect to the municipal lines on the north and west sides of the property.

Sanitary sewer service for the church is proposed to be provided via an 8" main which would be constructed across the central portion of the site and would connect to an existing 8 inch municipal sanitary sewer line located at the western boundary of the property. Sanitary sewer service for the rectory would be provided via connections to the existing 8 inch municipal sanitary sewer line located at the western boundary of the property. The City's Wastewater Treatment Plant (WWTP) has a capacity of 0.92 million gallons per day (mgd). Space remains for this proposed project and approximately 600 additional residential hook-ups. The City's recent residential project approvals exceed this amount and expansion of the plant is planned. The City will continue to monitor the WWTP on an annual basis to assess available capacity. The Phase 2 expansion of the WWTP will bring the capacity to 1.2 mgd. The timing of this expansion is not set. The Phase 2 expansion will need to take place before full build out of the residential units

With the applicant's agreement to accept and implement the following mitigation measures, this potential impact would be mitigated to a less than significant level by ensuring that adequate wastewater treatment capacity is available.

*Mitigation Measure Utilities 1 – The proposed systems for conveying project sewage, water, and drainage shall be finalized and approved by the City Engineer prior to final map. The project is required to fund and construct off-site improvements necessary to support the development. Such improvements could include, but not be limited to a water well, water lines, sewer lines and storm drainage lines. Should property acquisition or additional CEQA clearance be required for off-site improvements, this will be the responsibility of the developer.*

- c. The construction of impervious surfaces on the project site for the church, rectory and parking development would incrementally increase storm water runoff in the project vicinity. Stormwater drainage from the project site would be conveyed to the existing storm drainage main in West Main Street. The existing storm drainage system is designed to sufficiently handle the stormwater capacity that the project would create during a 100-year flood. Therefore, the project would not result in additional environmental effects beyond those analyzed in this document. This is a less than significant impact.
- d. The proposed project would be served by the City of Winters, which uses groundwater for municipal water supply. The City of Winters currently operates five groundwater wells to meet urban demand for water. Over the last ten years the City's pumping has ranged from a low of 1,540 acre-feet in 1995 to a high of 1,830 acre-feet in 2003. In 2003, production from the five wells dropped again to 1,565 acre-feet. In addition to the City's pumping, local agriculture, three local industries, one commercial enterprise, and several rural residences also pump water from the aquifer underlying the General Plan boundary. Over the last two years this additional pumping totaled approximately 90 acre-feet/year on top of the City's

pumping. In summary, currently between 1,655 and 1,920 acre-feet per year of groundwater are pumped to serve uses within the General Plan boundary. This compares to pumping in 1990 of about 2,660 acre-feet. The difference is due to whether or not surface water was available for agriculture. When less surface water is available, as was the case in 1990, there is greater groundwater pumping by agriculture.

By 2020, demand for groundwater within the City is estimated to increase to 3,620 acre-feet per year unrestricted and 3,250 acre-feet per year assuming a conservation scenario of six percent. The Proposed Project is estimated to generate a demand for municipal water of 6.42 acre-feet of water annually as shown in the table below.

Per the City Engineer the project would require 2,000 gallons per day per acre for a total site demand of 11,120 gallons per day. The increment of pumping needed to serve the proposed project would be available and would not adversely affect groundwater levels or storage underlying the City. This impact is less than significant. However, analysis for the City's Water Master Plan Update recommends that a new well will be required for any future development in the City. The City is in the process of bidding out the construction of a new that will be located near the intersection of West Grant Avenue and West Main Street.

With the applicant's agreement to accept and implement the following mitigation measure, the potential for impact associated with water supply and infrastructure will be mitigated to a less than significant level.

***Mitigation Measure Utilities 2 – A Certificate of Occupancy shall be issued only after the City Engineer has established that water supply will be available to serve the building.***

- f, g. Solid waste from the project site will be collected by the City of Winters and disposed of at the Yolo County Central Landfill, a 722-acre facility. The landfill has a capacity of 11 million tons with capacity for planned growth through 2025. The proposed project site has been planned for development since at least 1992. This project is part of the planned growth for which the landfill has been sized and therefore solid waste generated by the project would not have unanticipated impacts on the life of the landfill. Therefore, this impact is considered less than significant.

| Issues   | Potentially Significant Impact | Potentially Significant Unless Mitigation Incorporated | Less-Than-Significant Impact        | No Impact                |
|--|--------------------------------|--|-------------------------------------|--------------------------|
| <b>17. MANDATORY FINDINGS OF SIGNIFICANCE.</b>   |                                |  |                                     |                          |
| a. Does the project have the potential to degrade the quality of the environment, substantially reduce the habitat of a fish or wildlife species, cause a fish or wildlife population to drop below self-sustaining levels, threaten to eliminate a plant or animal community, reduce the number or restrict the range of a rare or endangered plant or animal or eliminate important examples of the major periods of California history or prehistory? | <input type="checkbox"/>       | <input checked="" type="checkbox"/>                    | <input type="checkbox"/>            | <input type="checkbox"/> |
| b. Does the project have impacts that are individually limited, but cumulatively considerable? ("Cumulatively considerable" means that the incremental effects of a project are considerable when viewed in connection with the effects of past projects, the effects of other current projects, and the effects of probable future projects)?   | <input type="checkbox"/>       | <input type="checkbox"/>                               | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| c. Does the project have environmental effects which will cause substantial adverse effects on human beings, either directly or indirectly?  | <input type="checkbox"/>       | <input checked="" type="checkbox"/>                    | <input type="checkbox"/>            | <input type="checkbox"/> |

**Discussion**

- a. No important examples of major periods of California history or prehistory in California were identified, and mitigation identified in Section 5 would ensure that subsurface resources, if present, would be protected. No wetlands or habitat for special-status species were identified on-site. Mitigation measures provided under Section 4 (Biological Resources) and Section 5 (Cultural Resources) of this Initial Study would ensure that impacts on biological resources would be less than significant.
- b. As discussed throughout this Initial Study, the proposed project is consistent with the Winters General Plan and assumptions made in the Winters General Plan EIR. Therefore cumulative impacts as analyzed in the 1992 General Plan EIR remain valid, and this project would not result in new or increased cumulative effects.
- c. As discussed in Sections 1 (Aesthetics), 3 (Air Quality), 6 (Geology and Soils), 9 (Land Use Planning), 11 (Noise), and 16 (Utilities and Service Systems) the potential for impacts on human beings would be reduced to less than significant levels by mitigation identified in these sections.

## Summary of Mitigation Measures

**Mitigation Measure Aesthetics 1** – Outdoor light fixtures shall be low-intensity, shielded and/or directed away from adjacent areas and the night sky. All light fixtures shall be installed and shielded in such a manner that no light rays are emitted from the fixture at angles above the horizontal plane. Lighting plans with certification that adjacent areas will not be adversely affected and that offsite illumination will not exceed 2-foot candles shall be submitted to the City for review and approval as part of improvement plans.

Prior to issuance of a building permit, the applicant shall submit a photometric and proposed lighting plan for the project to the satisfaction of the Community Development Department to ensure no spillover light and glare onto adjoining properties.

### **Mitigation Measure Air 1**

- a. Construction equipment exhaust emissions shall not exceed District Rule 2-11 Visible Emission limitations.
- b. Construction equipment shall minimize idling time to 5 minutes or less. Catalyst and filtration technologies shall be incorporated where feasible.
- c. The prime contractor shall submit to the District a comprehensive inventory (i.e. make, model, year, emission rating) of all the heavy-duty off-road equipment (50 horsepower or greater) that will be used an aggregate of 40 or more hours for the construction project. District personnel, with assistance from the California Air Resources Board, will conduct initial Visible Emission Evaluations of all heavy-duty equipment on the inventory list.

An enforcement plan shall be established to weekly evaluate project-related on-and-off-road heavy-duty vehicle engine emission opacities, using standards as defined in California Code of Regulations, Title 13, Sections 2180 - 2194. An Environmental Coordinator, CARB-certified to perform Visible Emissions Evaluations (VEE), shall routinely evaluate project related off-road and heavy duty on-road equipment emissions for compliance with this requirement. Operators of vehicles and equipment found to exceed opacity limits will be notified and the equipment must be repaired within 72 hours.

Construction contracts shall stipulate that at least 20% of the heavy-duty off-road equipment included in the inventory be powered by CARB certified off-road engines, as follows:

|                 |                        |
|-----------------|------------------------|
| 175 hp - 750 hp | 1996 and newer engines |
| 100 hp - 174 hp | 1997 and newer engines |
| 50 hp- 99 hp    | 1998 and newer engines |

In lieu of or in addition to this requirement, the applicant may use other measures to reduce particulate matter and nitrogen oxide emissions from project construction through the use of emulsified diesel fuel and or particulate matter traps. These alternative measures, if proposed, shall be developed in consultation with District staff.

### **Mitigation Measure Air 2**

- a. *Nontoxic soil stabilizers according to manufacturer's specifications shall be applied to all inactive construction areas (previously graded areas inactive for ten days or more).*
- b. *Ground cover shall be reestablished in disturbed areas quickly.*
- c. *Active construction sites shall be watered at least three times daily to avoid visible dust plumes.*
- d. *Paving, applying water three times daily, or applying (non-toxic) soil stabilizers shall occur on all unpaved access roads, parking areas and staging areas at construction sites*
- e. *Enclosing, covering, watering daily, or applying non-toxic soil binders to exposed stockpiles (dirt, sand, etc.) shall occur.*
- f. *A speed limit of 15 MPH for equipment and vehicles operated on unpaved areas shall be enforced.*
- g. *All vehicles hauling dirt, sand, soil, or other loose materials shall be covered or shall be maintained at least two feet of freeboard.*
- h. *Streets shall be swept at the end of the day if visible soil material is carried onto adjacent public paved roads.*

**Mitigation Measure Biological 1** – *The project proponent shall mitigate for potential project-related impacts to burrowing owl by conducting a pre-construction survey no more than 30 days prior to the initiation of construction activity. The pre-construction survey shall be conducted by a qualified biologist familiar with the identification of burrowing owls and the signs of burrowing owl activity. If active burrows are found on the project site, the California Department of Fish and Game (CDFG) shall be consulted regarding appropriate mitigation measures for project-related impacts to burrowing owl. Pursuant to the CDFG document entitled "Staff Report on Burrowing Owl Mitigation" (September 25, 1995), it is likely that replacement habitat will be required by CDFG. The guidelines include specific mitigation to protect nesting and wintering owls and to compensate for loss of breeding sites. In general, if the project would remove habitat of an occupied breeding site (e.g., if an active nest and surrounding habitat are removed), the project proponent will be required to compensate by preserving equivalent suitable habitat for each active nest site. In addition, the project proponent must install artificial burrows to offset the direct loss of the breeding site. Mitigation shall be consistent with the City's adopted Habitat Mitigation Program. Implementation of this mitigation measure shall be confirmed by the City of Winters prior to the initiation of construction activity.*

**Mitigation Measure Biological 2** – *The project proponent shall mitigate for potential project-related impacts to nesting raptors (Swainson's Hawk, White-tailed Kite, Northern Harrier, and Loggerhead Shrike) by conducting a pre-construction survey of all trees suitable for use by nesting raptors on the subject property or within 0.25 mile of the project boundary as allowable. The preconstruction survey shall be performed no more than 30 days prior to the implementation of construction activities. The preconstruction survey shall be conducted by a qualified biologist familiar with the identification of raptors known to occur in the vicinity of the City of Winters. If active special-status raptor nests are found during the preconstruction survey, a 0.25-mile (1,320-feet) buffer zone shall be*

established around the nest and no construction activity shall be conducted within this zone during the raptor nesting season. The buffer zone shall be marked with flagging, construction lathe, or other means to mark the boundary of the buffer zone. All construction personnel shall be notified as to the existence of the buffer zone and to avoid entering the buffer zone during the nesting season. Implementation of this mitigation measure shall be confirmed by the City of Winters prior to the initiation of construction activity.

**Mitigation Measure Biological 3** – The project proponent shall mitigate for potential project-related impacts to migratory birds by conducting a pre-construction survey for nests on the site. The preconstruction survey shall be performed no more than 14 days prior to the onset of vegetation and/or tree removal. The preconstruction survey shall be conducted by a qualified biologist familiar with the identification of migratory bird known to occur in the vicinity of the City of Winters. If active migratory bird nest(s) are found onsite during the preconstruction survey, the nest(s) shall not be disturbed or removed until the young have fledged and the nest is no longer active. A buffer may be required. All construction personnel shall be notified as to the existence of the buffer zone and to avoid entering the buffer zone during the nesting season. Implementation of this mitigation measure shall be confirmed by the City of Winters prior to the initiation of construction activity.

Alternatively, potential impacts to nesting birds or unfledged young would be avoided if vegetation and/or tree removal occurred only between September 1 and January 21.

**Mitigation Measure Biological 4** – Any mitigation required shall be implemented in a manner consistent with requirements, purpose and intent of the City of Winters' Habitat Mitigation Program.

**Mitigation Measure Cultural 1** – If cultural resources (historic, archeological, paleontological, and/or human remains) are encountered during construction, workers shall not alter the materials or their context until an appropriately trained cultural resource consultant has evaluated the situation. Project personnel shall not collect cultural resources. Prehistoric resources include chert or obsidian flakes, projectile points, mortars, pestles, dark friable soil containing shell and bone dietary debris, heat-affected rock, or human burials. Historic resources include stone or adobe foundations or walls, structures and remains with square nails, and refuse deposits often in old wells and privies.

**Mitigation Measure Cultural 2** - Should human remains be discovered, no further site disturbance shall occur until the county coroner has determined that the remains are not subject to the provisions of Section 27491 of the Government Code or any other related provisions of law concerning investigation of the circumstances, manner and cause of any death, and the recommendations concerning the treatment and disposition of the human remains have been made to the person responsible for the excavation, in the manner provided in Section 5097.98 of the Public Resources Code. If the coroner determines that the remains are not subject to his or her authority and the remains are recognized to be those of a Native American, the coroner shall contact the Native American Heritage Commission within 24 hours.

**Mitigation Measure Geology 1** – The applicant shall submit a soils and geotechnical report upon submittal of the initial improvement plans package. The improvement plans shall be signed by the soils engineer for conformance to the geotechnical report prior to approval by the City.

**Mitigation Measure Land Use 1** – All aspects of the project shall be subject to design review to ensure compatibility with the surrounding area and satisfaction of the Community Design Guidelines and other applicable principles of good community design.

**Mitigation Measure Land Use 2** – The proposed project height and parking provisions are subject to approval of a Planned Development Overlay for the subject property.

**Mitigation Measures Noise 1** - The project applicant shall submit a construction noise mitigation plan to the City of Winters for review and approval. The plan shall depict the location of construction equipment and describe how noise would be mitigated through methods such as, but not limited to, locating stationary noise-generating equipment (such as pumps and generators) as far as possible from nearby noise-sensitive receptors. Where practicable, noise-generating equipment will be shielded from nearby noise-sensitive receptors by noise-attenuating buffers such as structures or haul trucks. Onsite noise sources such as heavy equipment located less than 200 feet from noise-sensitive receptors will be equipped with noise-reducing engine housings. Portable acoustic barriers able to attenuate at least 6 dB will be placed around noise-generating equipment located within 200 feet of existing residences. Water tanks and equipment storage, staging, and warm-up areas shall be located as far from noise-sensitive receptors as possible. All noise attenuation measures identified in the plan shall be incorporated into the project.

**Mitigation Measure Noise 2** - Construction activities shall adhere to the following noise requirements:

All construction equipment shall utilize noise reduction features (e.g., mufflers and engine shrouds) that are no less effective than those originally installed by the manufacturer.

Hours of construction shall comply with those established in Chapter 8.20.100 of the Winters Municipal Code. Those hours are weekdays from 7:00 a.m. through 7:00 p.m. Construction is prohibited on weekends and federal holidays.

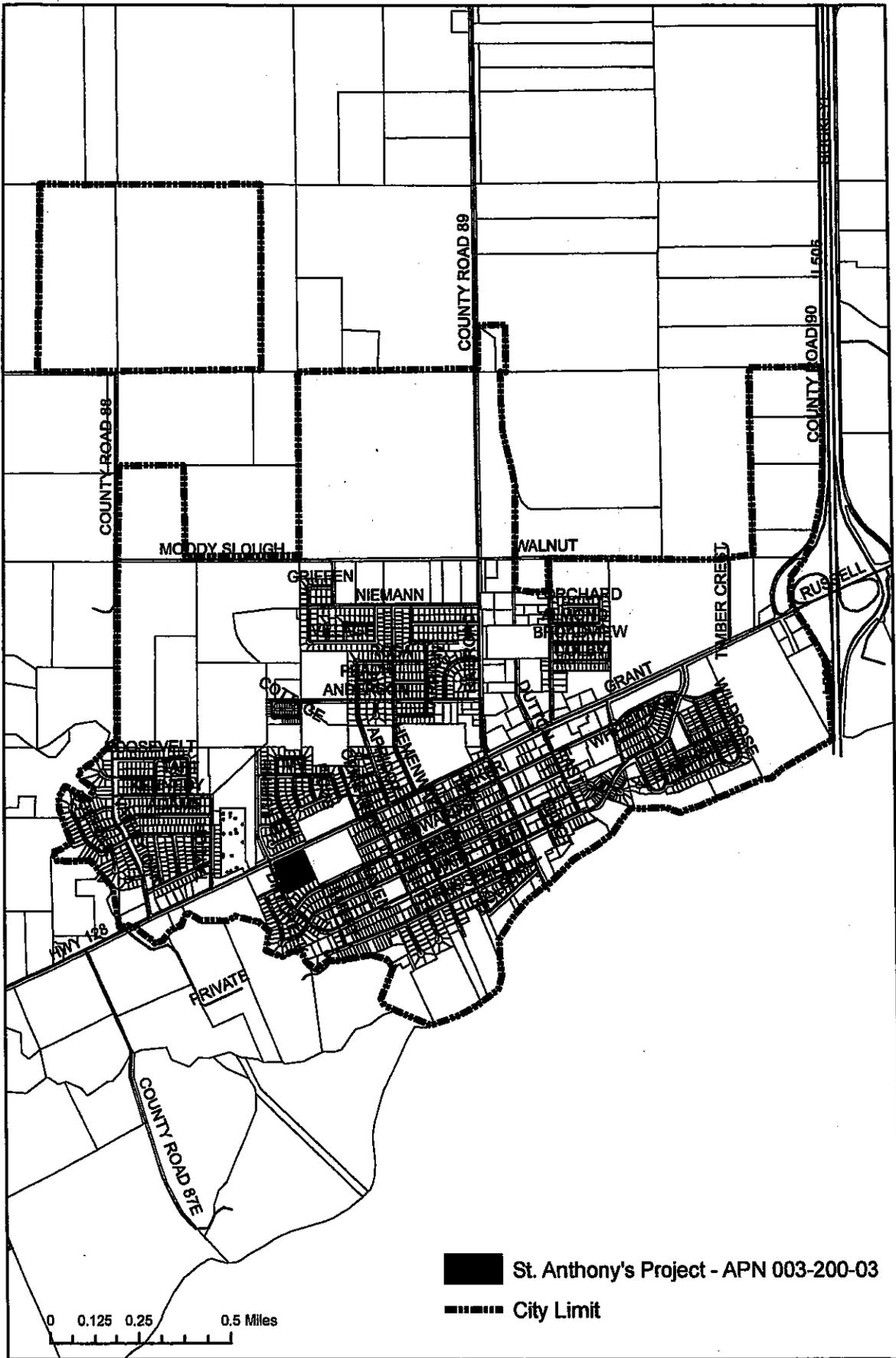
**Mitigation Measure Utilities 1** – The proposed systems for conveying project sewage, water, and drainage shall be finalized and approved by the City Engineer prior to approval of improvement plans. The project is required to fund and construct off-site improvements necessary to support the development. Such improvements could include, but not be limited to a water well, water lines, sewer lines and storm drainage lines. Should property acquisition or additional CEQA clearance be required for off-site improvements, this will be the responsibility of the developer.

***Mitigation Measure Utilities 2 – A Certificate of Occupancy shall be issued only after the City Engineer has established that water supply will be available to serve the building.***

**Attachments:**

1. Location Map
2. Site Plan
3. Floor Plans
4. Building Elevations
5. Landscape Plan
6. Mitigation Monitoring Plan (MMP)

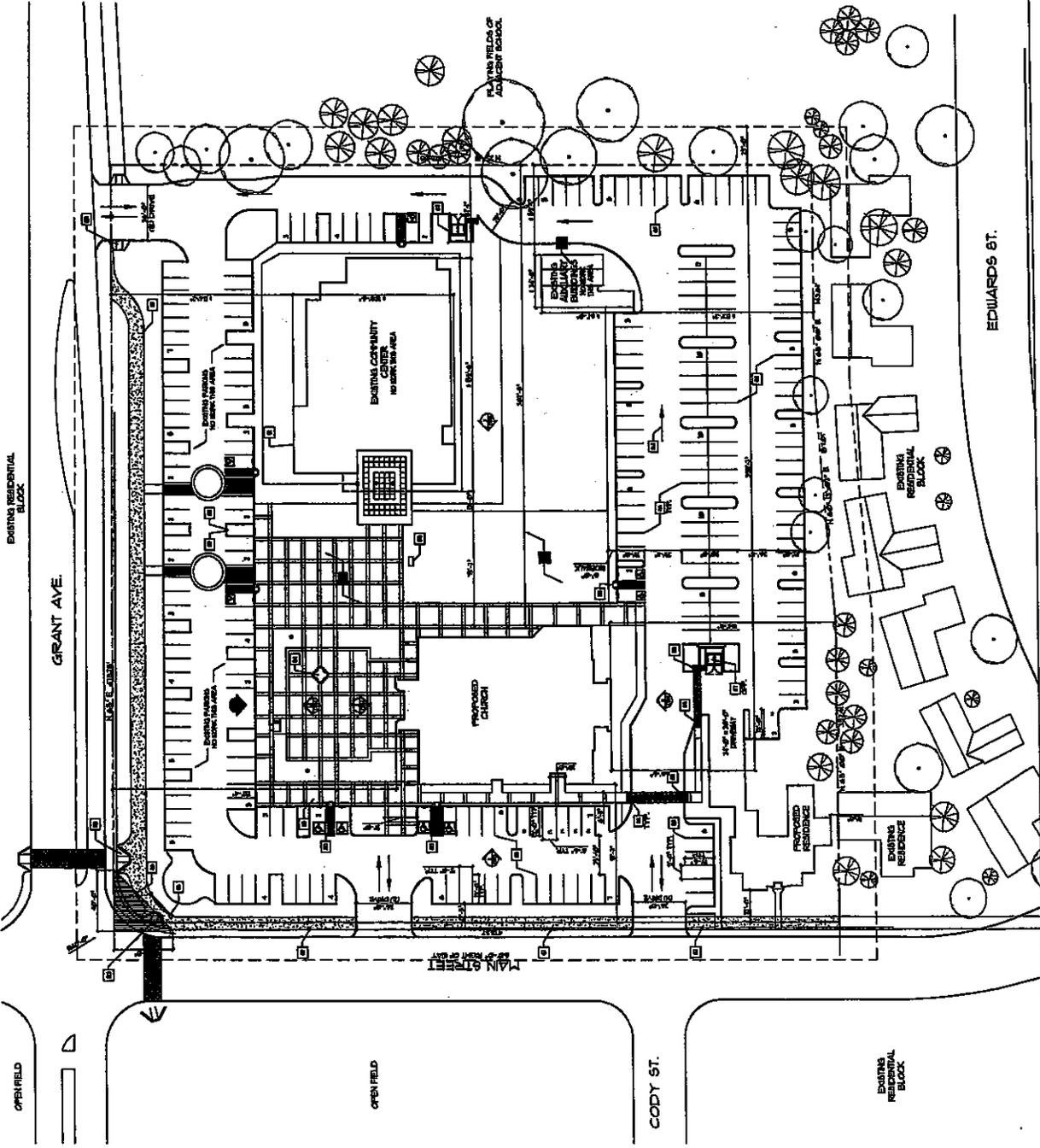
# St Anthony's Project - APN 003-200-03





- KEY NOTES:**
- 1 EXISTING BUILDINGS TO REMAIN
  - 2 NEW BUILDINGS
  - 3 EXISTING DRIVEWAY
  - 4 EXISTING DRIVEWAY
  - 5 EXISTING DRIVEWAY
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  - 100 EXISTING DRIVEWAY

- SITE LEGEND:**
- C EXISTING PROPERTY LINE
  - COMPACT PARKING STALL
  - INDIVIDUAL SIGN OF ACCESSIBILITY
  - NEW



1 ADJACENT LAND USE PLAN

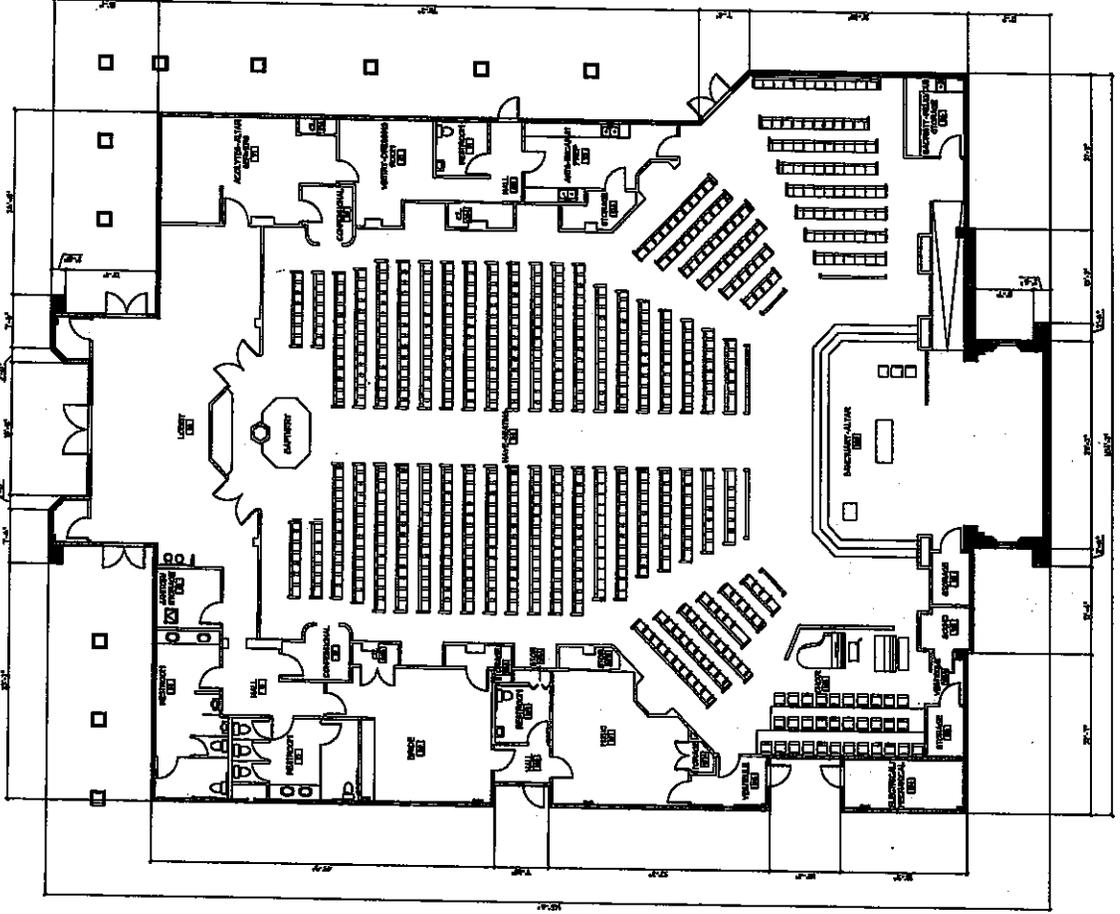


DRAIN  
CHIEF  
DRAIN  
TECH  
JOB #



PROPOSED PROJECT

LEGEND:  
DIMENSIONED ELEMENTS  
UNFINISHED WALL  
FINISHED WALL



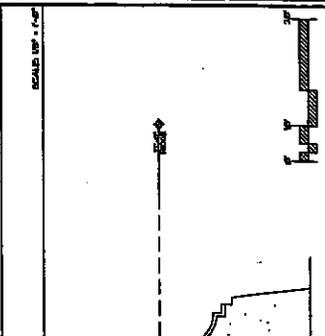
1 FLOOR PLAN

SCALE: 1/8" = 1'-0"

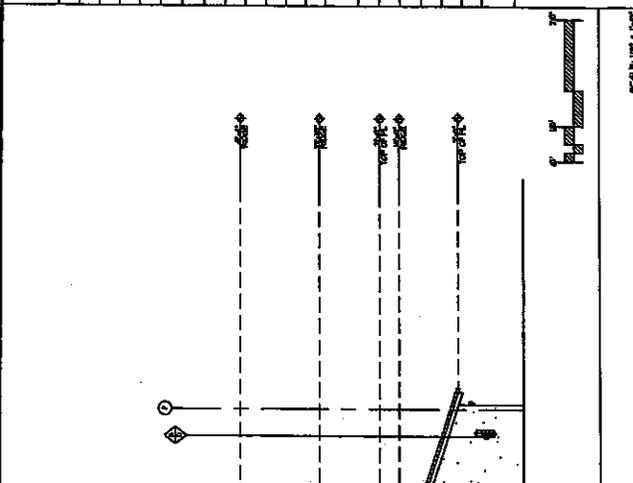


| MATERIAL |                                       |
|----------|---------------------------------------|
| 1        | GLAZED TO MATCH EXISTING WINDOW SIZES |
| 2        | WALL FINISH                           |
| 3        | ROOF                                  |
| 4        | TRIAL WINDOW                          |
| 5        | GLASS                                 |
| 6        | FINISH                                |
| 7        | GLAZED TO MATCH EXISTING WINDOW SIZES |
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| 9        | ROOF                                  |
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| 11       | GLASS                                 |
| 12       | FINISH                                |
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| 102      | FINISH                                |
| 103      | GLAZED TO MATCH EXISTING WINDOW SIZES |
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| 112      | TRIAL WINDOW                          |
| 113      | GLASS                                 |
| 114      | FINISH                                |
| 115      | GLAZED TO MATCH EXISTING WINDOW SIZES |
| 116      | WALL FINISH                           |
| 117      | ROOF                                  |
| 118      | TRIAL WINDOW                          |
| 119      | GLASS                                 |
| 120      | FINISH                                |
| 121      | GLAZED TO MATCH EXISTING WINDOW SIZES |
| 122      | WALL FINISH                           |
| 123      | ROOF                                  |
| 124      | TRIAL WINDOW                          |
| 125      | GLASS                                 |
| 126      | FINISH                                |
| 127      | GLAZED TO MATCH EXISTING WINDOW SIZES |
| 128      | WALL FINISH                           |
| 129      | ROOF                                  |
| 130      | TRIAL WINDOW                          |
| 131      | GLASS                                 |
| 132      | FINISH                                |
| 133      | GLAZED TO MATCH EXISTING WINDOW SIZES |
| 134      | WALL FINISH                           |
| 135      | ROOF                                  |
| 136      | TRIAL WINDOW                          |
| 137      | GLASS                                 |
| 138      | FINISH                                |
| 139      | GLAZED TO MATCH EXISTING WINDOW SIZES |
| 140      | WALL FINISH                           |
| 141      | ROOF                                  |
| 142      | TRIAL WINDOW                          |
| 143      | GLASS                                 |
| 144      | FINISH                                |
| 145      | GLAZED TO MATCH EXISTING WINDOW SIZES |
| 146      | WALL FINISH                           |
| 147      | ROOF                                  |
| 148      | TRIAL WINDOW                          |
| 149      | GLASS                                 |
| 150      | FINISH                                |

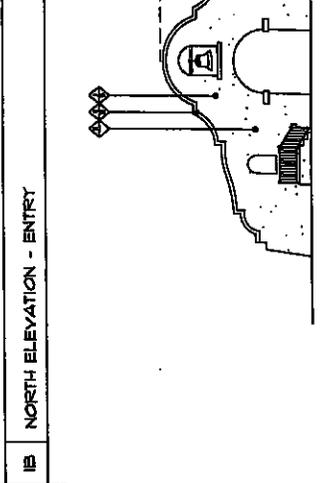
**NOTES:**  
 1. ALL ROOF JOISTS TO BE PAINTED B.  
 2. ALL ROOF JOISTS TO BE PAINTED B.  
 3. CEILING TO MATCH EXISTING PAINTED B.



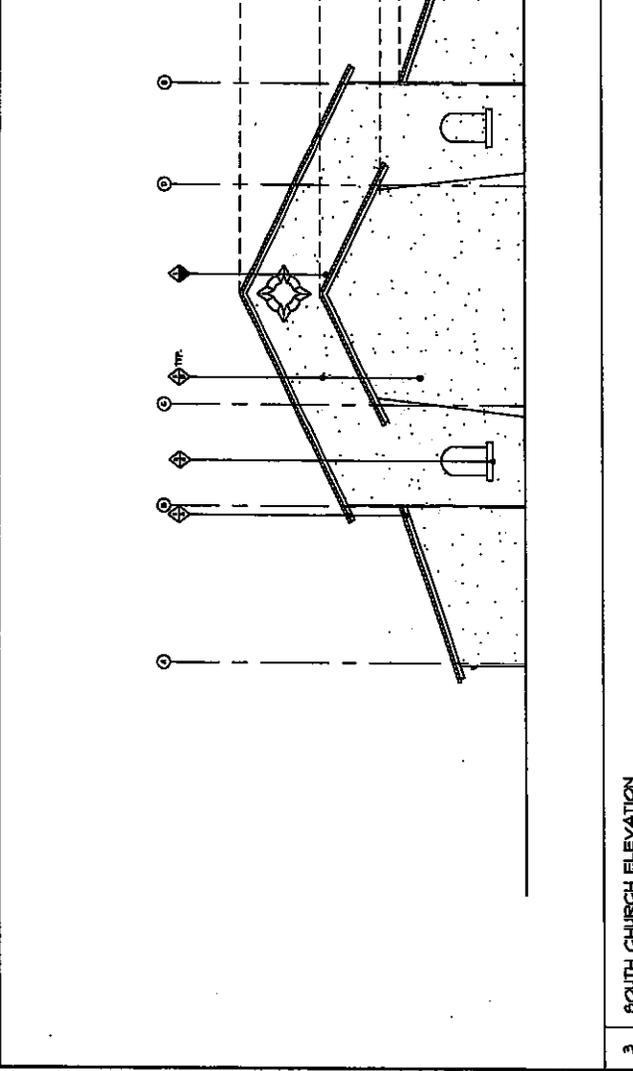
3 SOUTH CHURCH ELEVATION  
 SCALE: 1/8" = 1'-0"



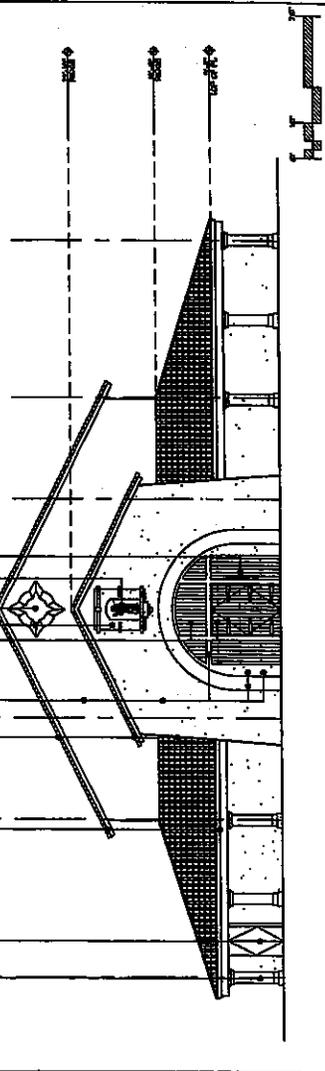
1B NORTH ELEVATION - ENTRY  
 SCALE: 1/8" = 1'-0"



1A SOUTH ELEVATION - ENTRY  
 SCALE: 1/8" = 1'-0"



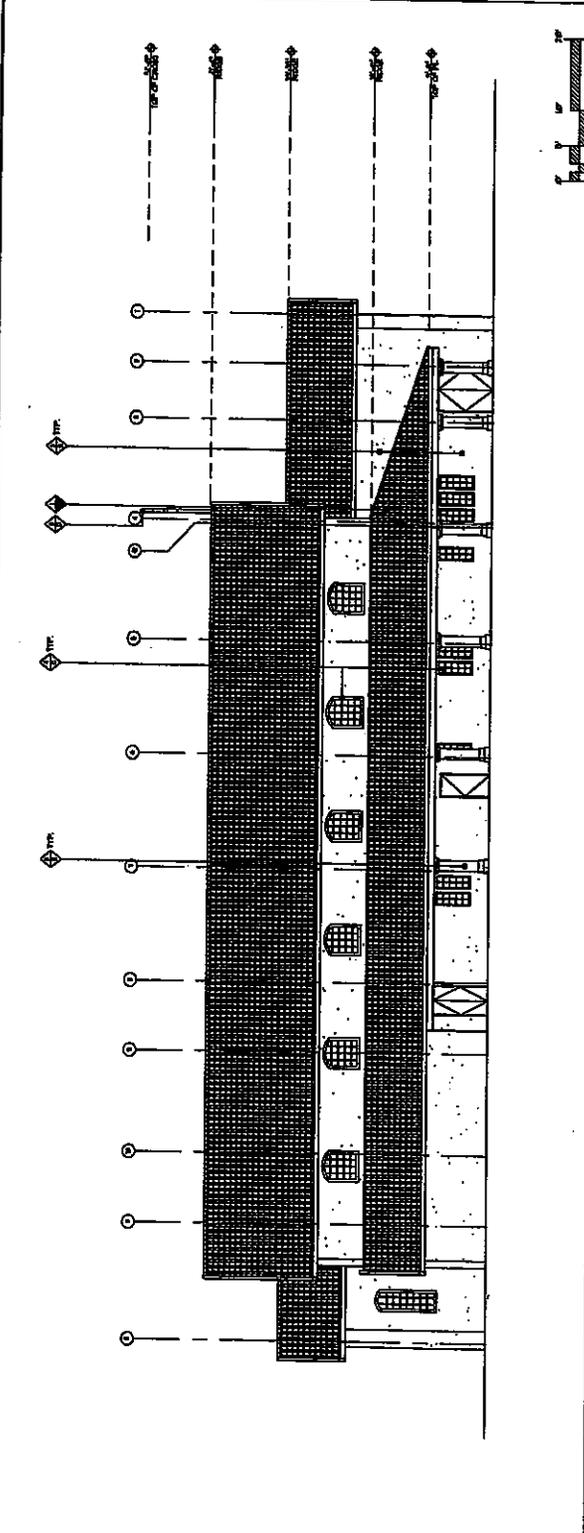
2 NORTH CHURCH ELEVATION  
 SCALE: 1/8" = 1'-0"



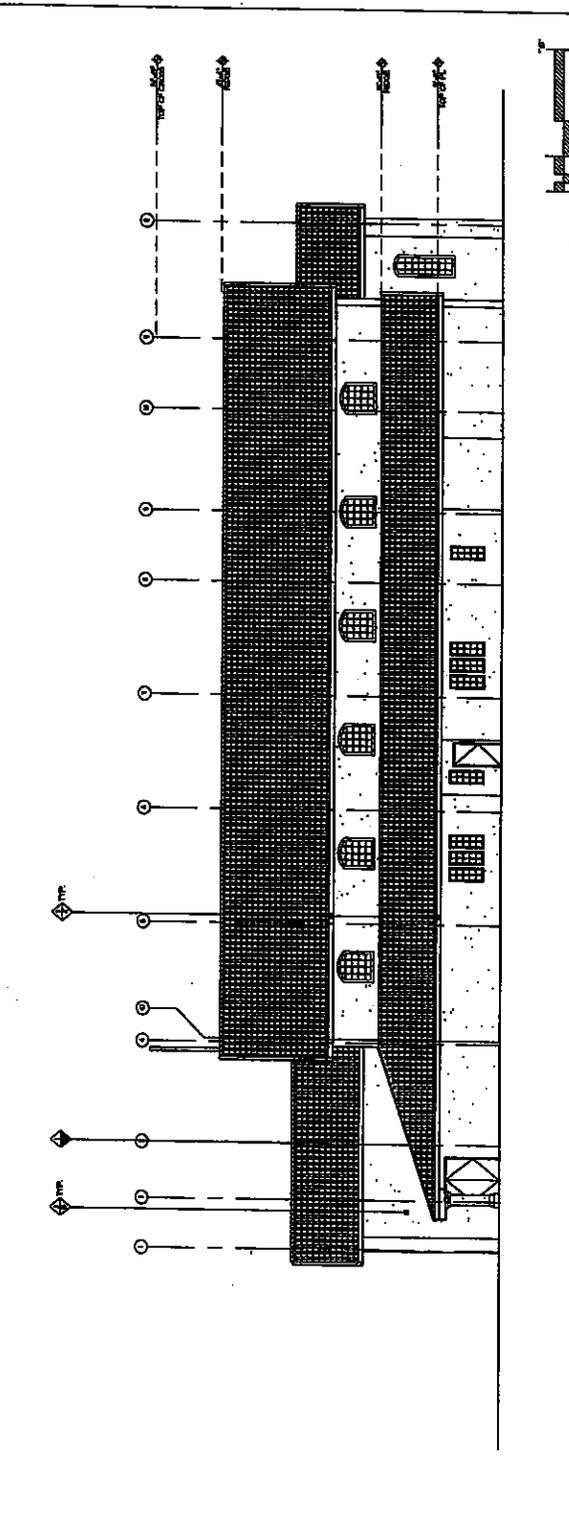
3 SOUTH CHURCH ELEVATION  
 SCALE: 1/8" = 1'-0"

| MATERIAL |                                       |
|----------|---------------------------------------|
| 1        | CLAY TILE TO MATCH EXISTING BUILDING  |
| 2        | PAINT / FINISH                        |
| 3        | BRICK                                 |
| 4        | TRIAL COLORS                          |
| 5        | COLOR                                 |
| 6        | STAINED WOOD DOOR                     |
| 7        | EXTERIOR WALL                         |
| 8        | INTERIOR WALL COVERING                |
| 9        | ROOFING                               |
| 10       | GLASS                                 |
| 11       | CONC.                                 |
| FINISH   |                                       |
| A        | GRANITE TILE WALLS INTERIOR OF CHURCH |
| B        | PAINT                                 |
| C        |                                       |
| D        |                                       |
| E        |                                       |
| F        |                                       |

1. BRICKS TO BE PAINTED A  
 2. ALL WOOD DOORS TO BE PAINTED A  
 3. ROOFING TO MATCH EXISTING BUILDING



2 WEST CHURCH ELEVATION



1 EAST CHURCH ELEVATION

**PLAN LEGEND:**

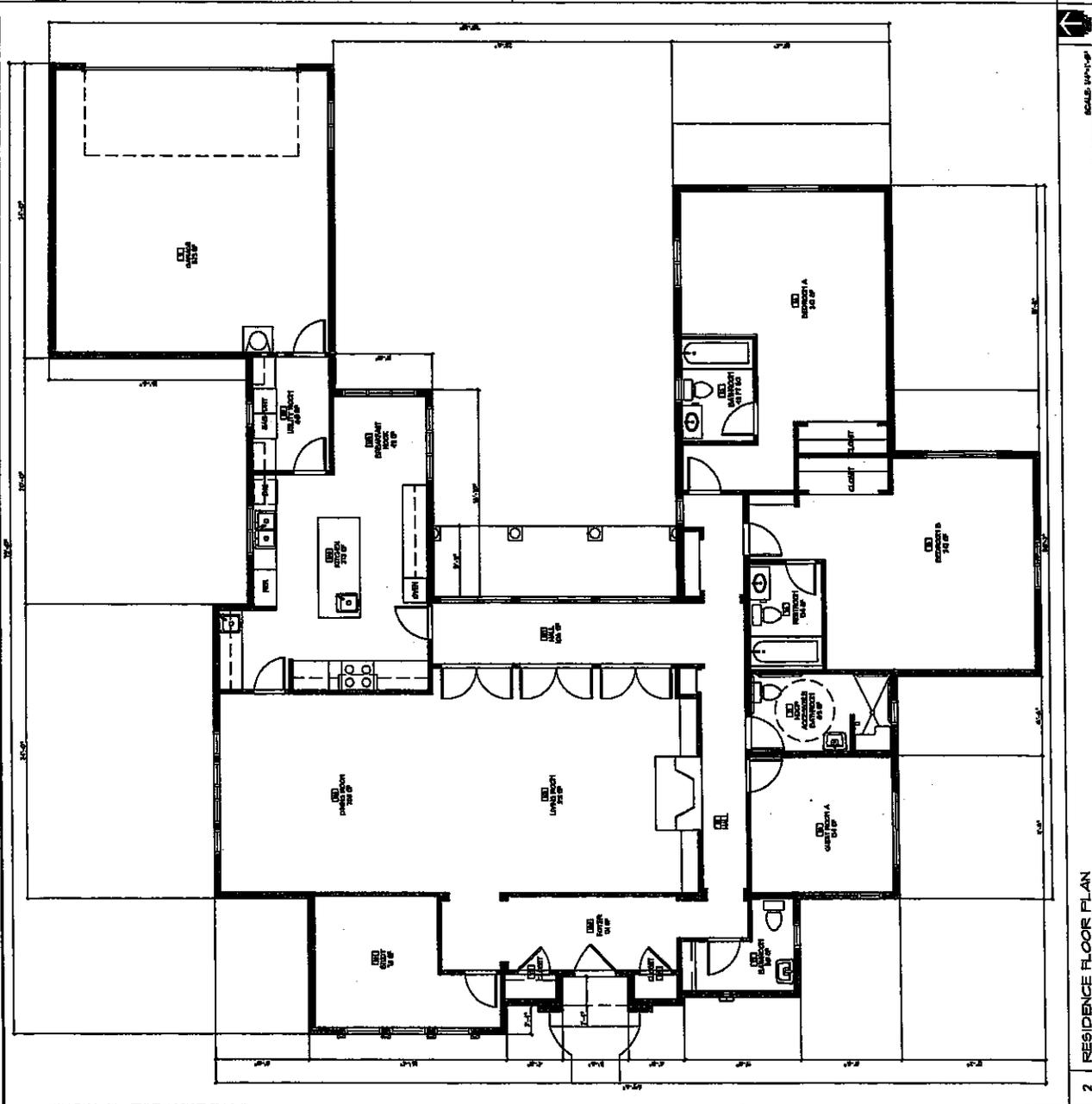
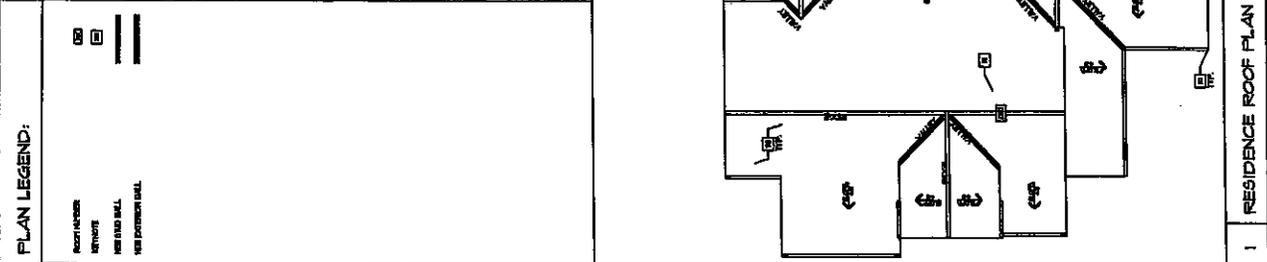
- ROOF FINISH
- ROOF INSULATION
- ROOF DRAIN
- ROOF GUTTER
- ROOF FLASHING
- ROOF CURB
- ROOF VALLEY
- ROOF PARAPET
- ROOF EAVES
- ROOF SLOPE
- ROOF RISE
- ROOF FALL
- ROOF DRAINAGE
- ROOF VENT
- ROOF ACCESS
- ROOF ESCAPE
- ROOF STAIR
- ROOF LIFT
- ROOF MECHANICAL
- ROOF ELECTRICAL
- ROOF PLUMBING
- ROOF HVAC
- ROOF LIGHTING
- ROOF SECURITY
- ROOF SAFETY
- ROOF MAINTENANCE
- ROOF REPAIR
- ROOF REPLACEMENT
- ROOF DEMOLITION
- ROOF CONSTRUCTION
- ROOF FINISHES
- ROOF MATERIALS
- ROOF COLORS
- ROOF TEXTURES
- ROOF PATTERNS
- ROOF SHAPES
- ROOF SIZES
- ROOF WEIGHTS
- ROOF STRENGTHS
- ROOF DURABILITY
- ROOF LIFESPANS
- ROOF WARRANTIES
- ROOF INSURANCES
- ROOF REGULATIONS
- ROOF STANDARDS
- ROOF CODES
- ROOF PERMITS
- ROOF INSPECTIONS
- ROOF APPROVALS
- ROOF CERTIFICATIONS
- ROOF ACCREDITATIONS
- ROOF REGISTRATIONS
- ROOF LICENSES
- ROOF BONDS
- ROOF COVERS
- ROOF PROTECTORS
- ROOF SHIELDS
- ROOF BARRIERS
- ROOF FENCES
- ROOF GATES
- ROOF DOORS
- ROOF WINDOWS
- ROOF VENTILATORS
- ROOF EXHAUSTERS
- ROOF FAN COILS
- ROOF CONDENSERS
- ROOF COMPRESSORS
- ROOF REFRIGERATORS
- ROOF FREEZERS
- ROOF COOLERS
- ROOF HEATERS
- ROOF BOILERS
- ROOF RADIATORS
- ROOF PUMPS
- ROOF VALVES
- ROOF FITTINGS
- ROOF JOINTS
- ROOF GASKETS
- ROOF O-RINGS
- ROOF WASHERS
- ROOF NUTS
- ROOF BOLTS
- ROOF SCREWS
- ROOF NAILS
- ROOF PINS
- ROOF WAGERS
- ROOF BRACKETS
- ROOF CLIPS
- ROOF ANCHORS
- ROOF FASTENERS
- ROOF CONNECTORS
- ROOF ADAPTERS
- ROOF TRANSFORMERS
- ROOF CONVERTERS
- ROOF EXTENDERS
- ROOF REDUCERS
- ROOF ENLARGERS
- ROOF TAPES
- ROOF GLUES
- ROOF SEALANTS
- ROOF ADHESIVES
- ROOF COATINGS
- ROOF PAINTS
- ROOF STAINERS
- ROOF PRESERVATIVES
- ROOF PROTECTANTS
- ROOF CLEANERS
- ROOF RESTORERS
- ROOF REPAIRERS
- ROOF MAINTENANCE PERSONNEL
- ROOF INSPECTORS
- ROOF ENGINEERS
- ROOF ARCHITECTS
- ROOF DESIGNERS
- ROOF CONSULTANTS
- ROOF SPECIALISTS
- ROOF EXPERTS
- ROOF ADVISORS
- ROOF REVIEWERS
- ROOF APPROVERS
- ROOF SIGNERS
- ROOF STAMPERS
- ROOF SEALERS
- ROOF LOCKERS
- ROOF UNLOCKERS
- ROOF OPENERS
- ROOF CLOSERS
- ROOF LOCKERS
- ROOF UNLOCKERS
- ROOF OPENERS
- ROOF CLOSERS

**ROOF KEY NOTES:**

- 1. SEE ELEVATIONS FOR COLOR / MATERIAL SELECTION
- 2. SEE ELEVATIONS FOR COLOR / MATERIAL SELECTION
- 3. SEE ELEVATIONS FOR COLOR / MATERIAL SELECTION
- 4. SEE ELEVATIONS FOR COLOR / MATERIAL SELECTION

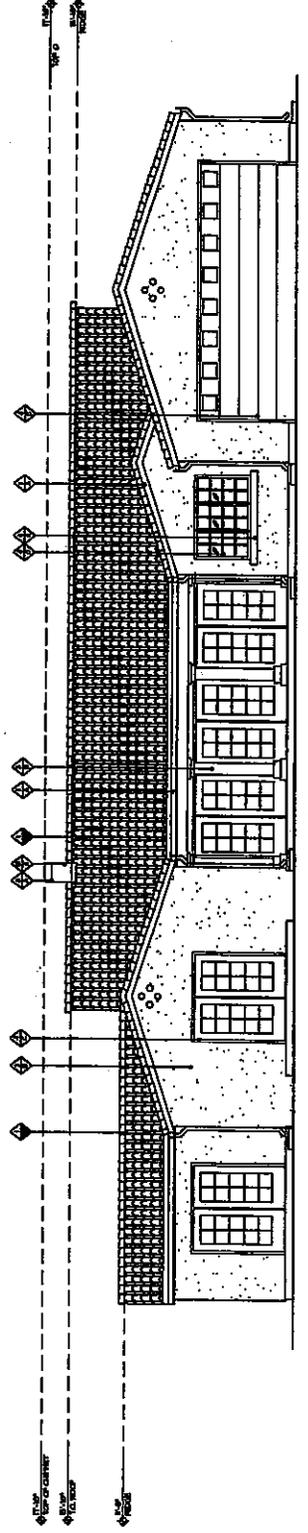
**ROOF GENERAL NOTES:**

1. ALL ROOF FINISHES AND MATERIALS SHALL BE INSTALLED IN ACCORDANCE WITH THE MANUFACTURER'S INSTRUCTIONS AND THE LATEST EDITIONS OF THE INTERNATIONAL BUILDING CODE (IBC) AND THE NATIONAL FIRE PROTECTION ASSOCIATION (NFPA) 90A AND 90B.

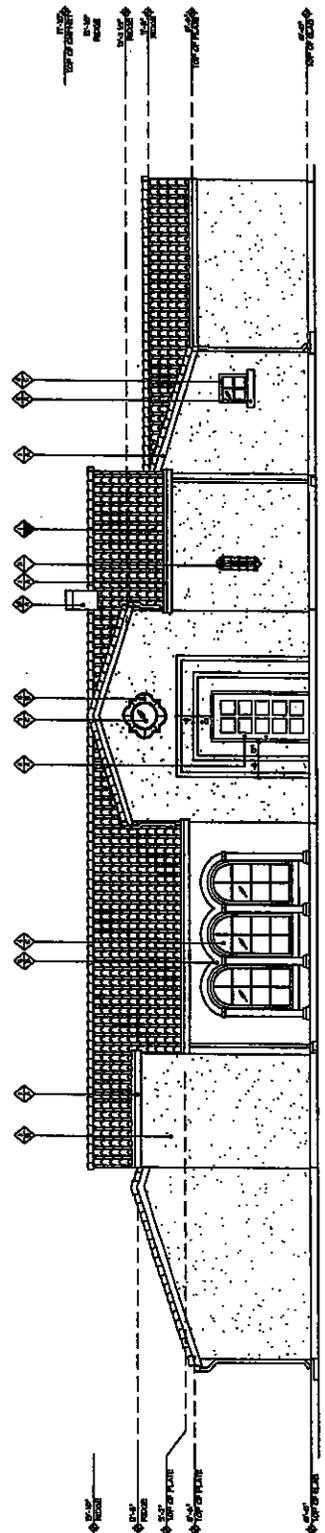


| MATERIAL |  |
|----------|--|
| 1        | CLADDING TO MATCH EXISTING ADJACENT BUILDING |
| 2        | BRICK  |
| 3        | WOOD   |
| 4        | METAL ROOFING                                |
| 5        | GLASS  |
| 6        | STAINED WOOD DOOR                            |
| 7        | WOOD TRIM                                    |
| 8        | WOOD TRIM                                    |
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| 100      | WOOD TRIM                                    |

**NOTES:**  
 1. UNLESS SPECIFIED TO BE PAINTED IN A  
 2. ALL WOODWORK TO BE PAINTED IN A  
 3. CLADDING TO MATCH EXISTING ADJACENT BUILDING.  
 4. ROOF SLATES ARE TYPICALLY AS SHOWN UNLESS SPECIFIED.



**EAST ELEVATION** SCALE: 1/4" = 1'-0"



**WEST ELEVATION** SCALE: 1/4" = 1'-0"



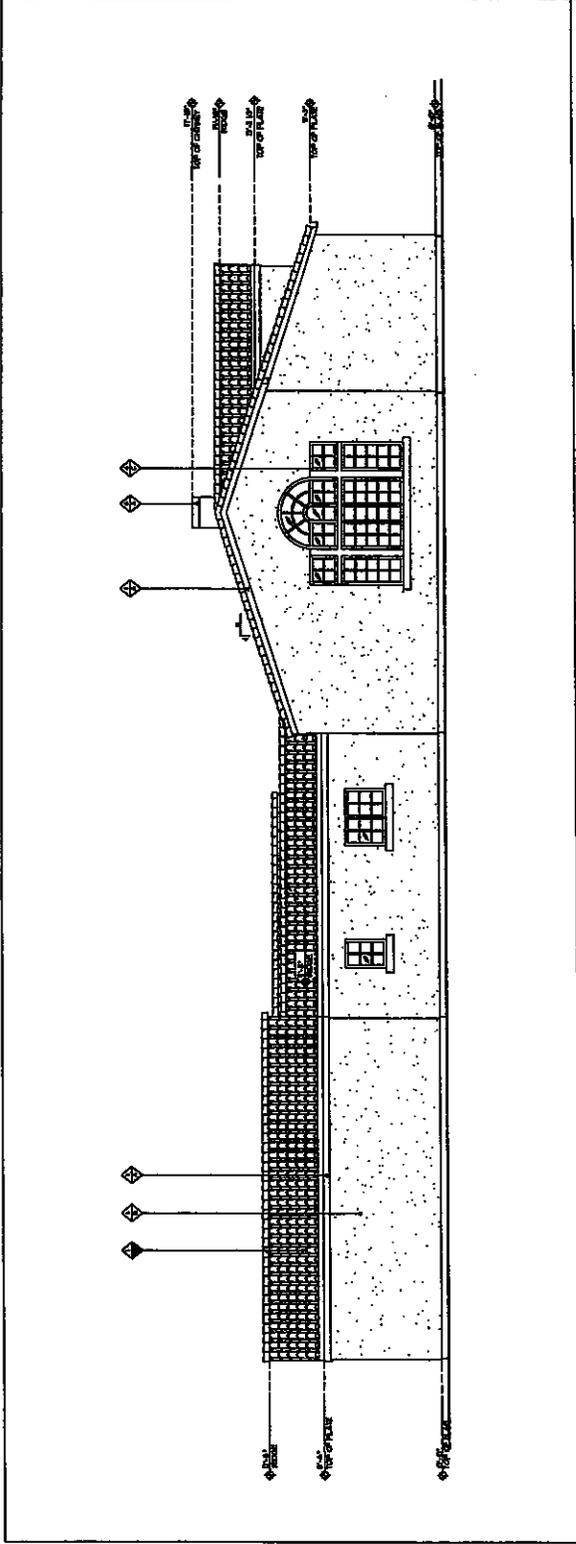
| NO. | DESCRIPTION                                   |
|-----|---|
| 1   | CLAY TILE TO MATCH EXISTING ROOFING MATERIALS |
| 2   | WALLS / FINISH                                |
| 3   | TRUSS   |
| 4   | TRUSS BRACES OR JOIST                         |
| 5   | CEILING                                       |
| 6   | GRANITE BLOCK JOINT                           |
| 7   | DECORATIVE MOULD                              |
| 8   | PLASTER                                       |
| 9   | PAINTWORK                                     |
| 10  | X   |
| 11  | GLAZING                                       |
| 12  | JOINT   |
| 13  | WOOD TRIM ELEMENT WITH CASE                   |

**FINISH**

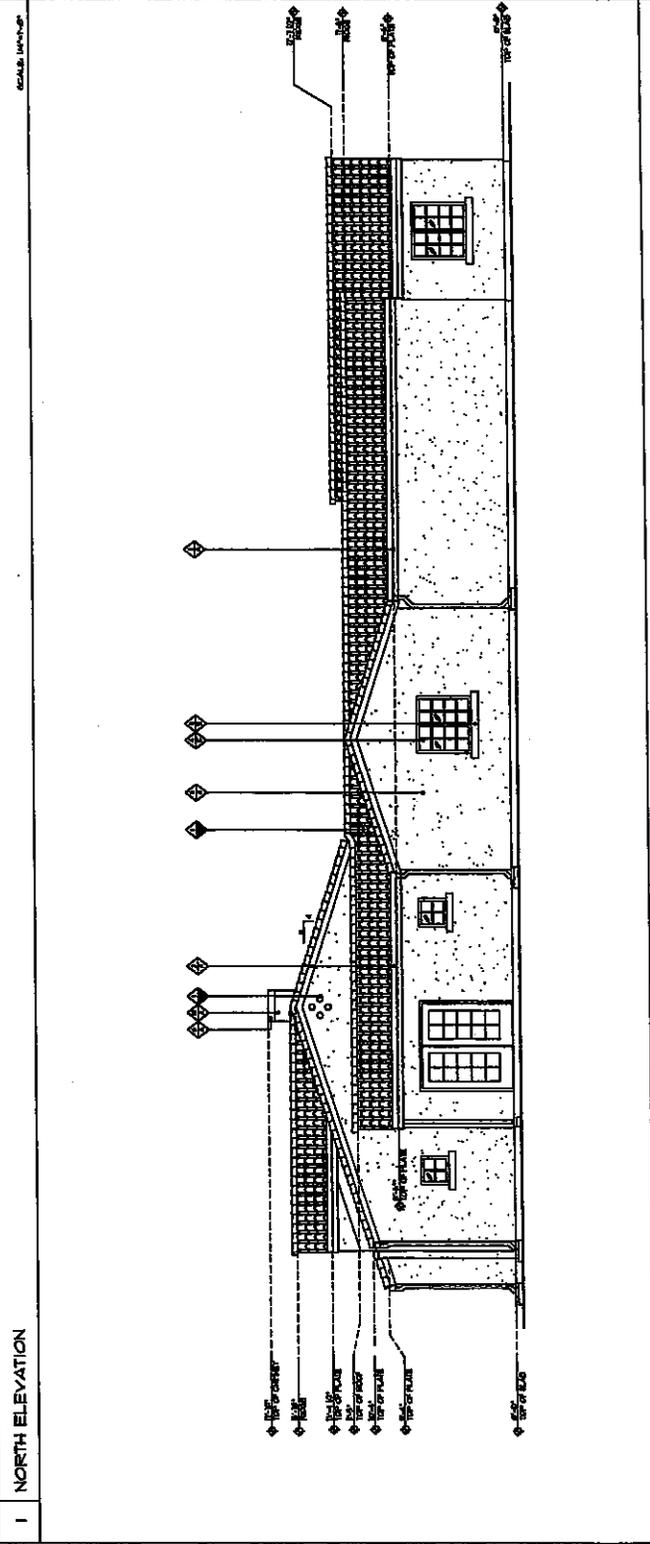
- A. DOWN EXTERIOR WALLS FINISH OF BRK
- B. DOWN EXTERIOR WALLS FINISH OF BRK
- C. GRANITE BLOCK
- D. BLACK
- E. WHITE VALLEY GATE BRICK
- F. CLEAN

**NOTES:**

- 1. BUILDING SHOULD BE PAINTED B
- 2. ALL WOOD SHOULD BE PAINTED A
- 3. ROOF SHOULD BE PAINTED A
- 4. ROOF BRACES ARE TYPICALLY 4X8 UNLESS OTHERWISE NOTED



1 NORTH ELEVATION



2 SOUTH ELEVATION

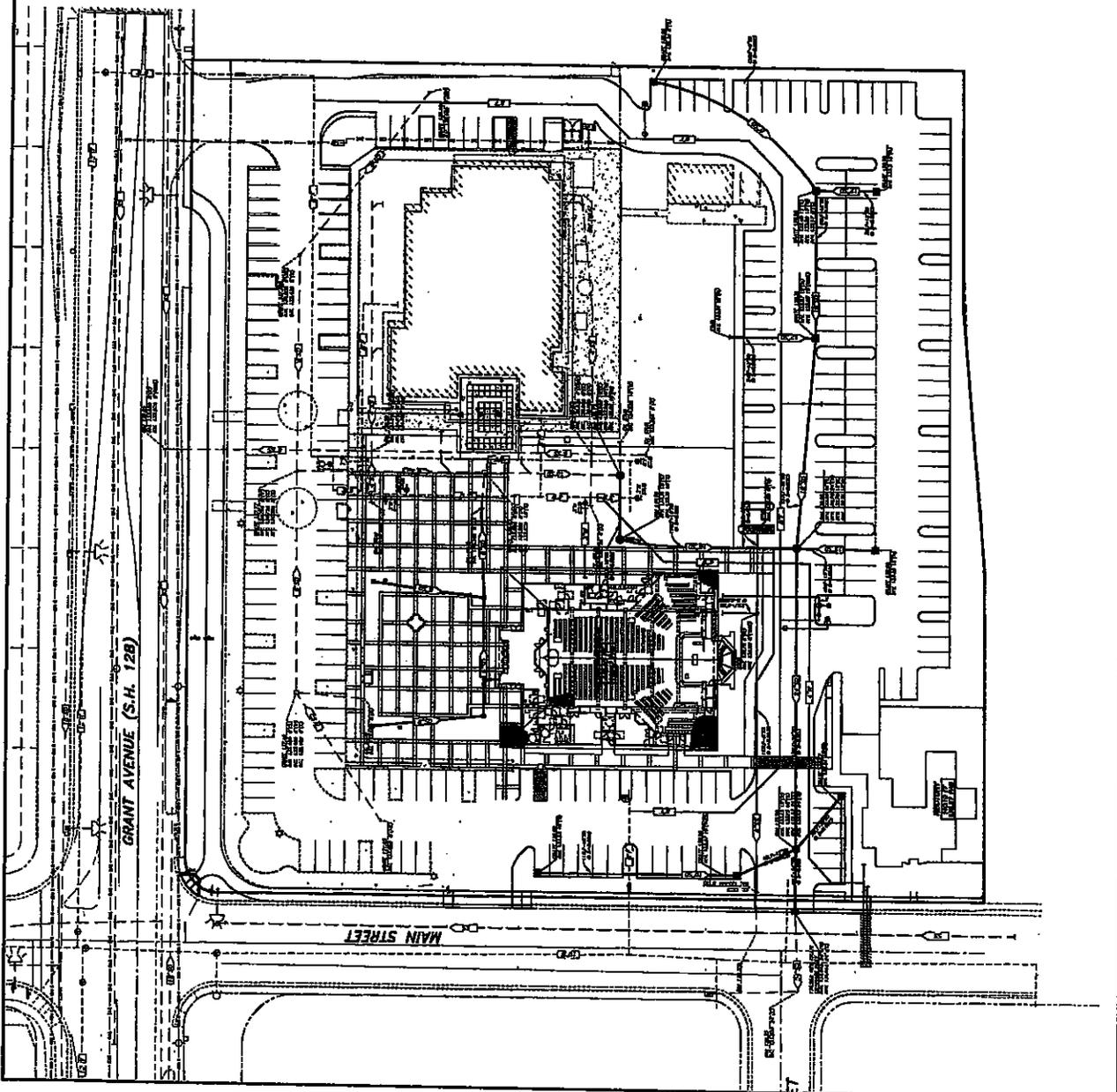
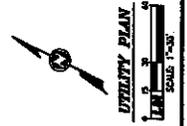
REVISIONS

ST. ANTHONY'S HOSPITAL  
PROPOSED PROJECT



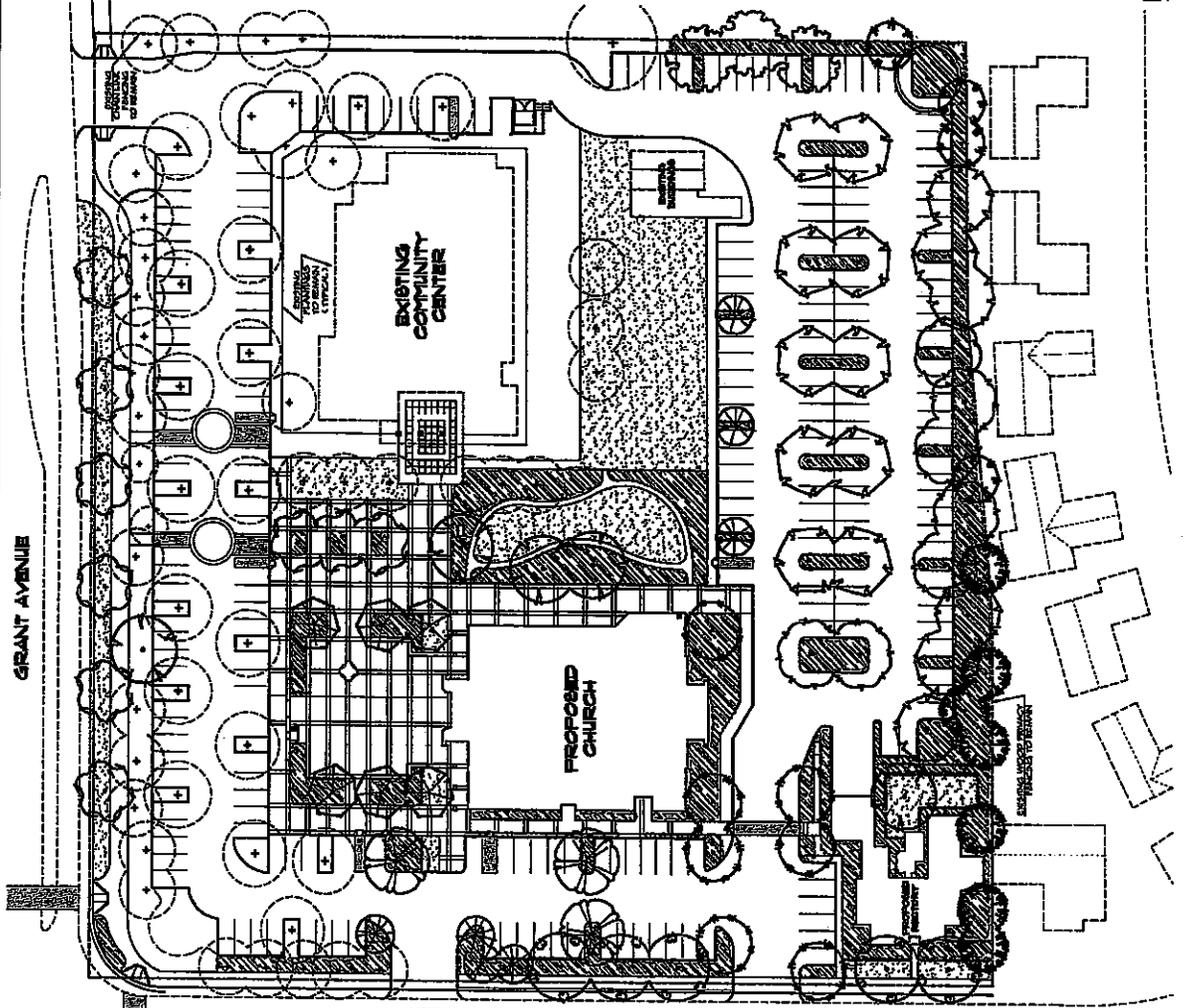
DESIGN REVIEW SHEET

|           |      |
|-----------|------|
| DRAWN     | DATE |
| CHECKED   | DATE |
| IN CHARGE | DATE |
| SCALE     | DATE |
| SHEET     | DATE |
| OF 3      | DATE |





**PLANT LIST & LEGEND**  
 1. 1" DIA. TREE  
 2. 2" DIA. TREE  
 3. 3" DIA. TREE  
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**POP-UP SPRINKLER DETAIL A**

**DRIP IRRIGATION DETAIL B**

**PARKING LOT SHADE CALCULATIONS:**

| TREE SPECIES       | QUANTITY | SHADE AREA (SQ. FT.) | TOTAL SHADE AREA (SQ. FT.) |
|--------------------|----------|----------------------|----------------------------|
| ACER FRAXINOSA     | 1        | 1127                 | 1127                       |
| ACER RUBRA         | 1        | 1127                 | 2254                       |
| ACER SACCHARINUM   | 1        | 1127                 | 3381                       |
| ACER SPICATUM      | 1        | 1127                 | 4508                       |
| ALNUS INCANA       | 1        | 1127                 | 5635                       |
| AMALANCHA ALBA     | 1        | 1127                 | 6762                       |
| AMALANCHA CATENATA | 1        | 1127                 | 7889                       |
| AMALANCHA SPICATA  | 1        | 1127                 | 9016                       |
| AMALANCHA SPICATA  | 1        | 1127                 | 10143                      |
| AMALANCHA SPICATA  | 1        | 1127                 | 11270                      |
| AMALANCHA SPICATA  | 1        | 1127                 | 12397                      |
| AMALANCHA SPICATA  | 1        | 1127                 | 13524                      |
| AMALANCHA SPICATA  | 1        | 1127                 | 14651                      |
| AMALANCHA SPICATA  | 1        | 1127                 | 15778                      |
| AMALANCHA SPICATA  | 1        | 1127                 | 16905                      |
| AMALANCHA SPICATA  | 1        | 1127                 | 18032                      |
| AMALANCHA SPICATA  | 1        | 1127                 | 19159                      |
| AMALANCHA SPICATA  | 1        | 1127                 | 20286                      |
| AMALANCHA SPICATA  | 1        | 1127                 | 21413                      |
| AMALANCHA SPICATA  | 1        | 1127                 | 22540                      |
| AMALANCHA SPICATA  | 1        | 1127                 | 23667                      |
| AMALANCHA SPICATA  | 1        | 1127                 | 24794                      |
| AMALANCHA SPICATA  | 1        | 1127                 | 25921                      |
| AMALANCHA SPICATA  | 1        | 1127                 | 27048                      |
| AMALANCHA SPICATA  | 1        | 1127                 | 28175                      |
| AMALANCHA SPICATA  | 1        | 1127                 | 29302                      |
| AMALANCHA SPICATA  | 1        | 1127                 | 30429                      |
| AMALANCHA SPICATA  | 1        | 1127                 | 31556                      |
| AMALANCHA SPICATA  | 1        | 1127                 | 32683                      |
| AMALANCHA SPICATA  | 1        | 1127                 | 33810                      |
| AMALANCHA SPICATA  | 1        | 1127                 | 34937                      |
| AMALANCHA SPICATA  | 1        | 1127                 | 36064                      |
| AMALANCHA SPICATA  | 1        | 1127                 | 37191                      |
| AMALANCHA SPICATA  | 1        | 1127                 | 38318                      |
| AMALANCHA SPICATA  | 1        | 1127                 | 39445                      |
| AMALANCHA SPICATA  | 1        | 1127                 | 40572                      |
| AMALANCHA SPICATA  | 1        | 1127                 | 41699                      |
| AMALANCHA SPICATA  | 1        | 1127                 | 42826                      |
| AMALANCHA SPICATA  | 1        | 1127                 | 43953                      |
| AMALANCHA SPICATA  | 1        | 1127                 | 45080                      |
| AMALANCHA SPICATA  | 1        | 1127                 | 46207                      |
| AMALANCHA SPICATA  | 1        | 1127                 | 47334                      |
| AMALANCHA SPICATA  | 1        | 1127                 | 48461                      |
| AMALANCHA SPICATA  | 1        | 1127                 | 49588                      |
| AMALANCHA SPICATA  | 1        | 1127                 | 50715                      |
| AMALANCHA SPICATA  | 1        | 1127                 | 51842                      |
| AMALANCHA SPICATA  | 1        | 1127                 | 52969                      |
| AMALANCHA SPICATA  | 1        | 1127                 | 54096                      |
| AMALANCHA SPICATA  | 1        | 1127                 | 55223                      |
| AMALANCHA SPICATA  | 1        | 1127                 | 56350                      |
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| AMALANCHA SPICATA  | 1        | 1127                 | 63112                      |
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| AMALANCHA SPICATA  | 1        | 1127                 | 66493                      |
| AMALANCHA SPICATA  | 1        | 1127                 | 67620                      |
| AMALANCHA SPICATA  | 1        | 1127                 | 68747                      |
| AMALANCHA SPICATA  | 1        | 1127                 | 69874                      |
| AMALANCHA SPICATA  | 1        | 1127                 | 71001                      |
| AMALANCHA SPICATA  | 1        | 1127                 | 72128                      |
| AMALANCHA SPICATA  | 1        | 1127                 | 73255                      |
| AMALANCHA SPICATA  | 1        | 1127                 | 74382                      |
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| AMALANCHA SPICATA  | 1        | 1127                 | 82271                      |
| AMALANCHA SPICATA  | 1        | 1127                 | 83398                      |
| AMALANCHA SPICATA  | 1        | 1127                 | 84525                      |
| AMALANCHA SPICATA  | 1        | 1127                 | 85652                      |
| AMALANCHA SPICATA  | 1        | 1127                 | 86779                      |
| AMALANCHA SPICATA  | 1        | 1127                 | 87906                      |
| AMALANCHA SPICATA  | 1        | 1127                 | 89033                      |
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| AMALANCHA SPICATA  | 1        | 1127                 | 94668                      |
| AMALANCHA SPICATA  | 1        | 1127                 | 95795                      |
| AMALANCHA SPICATA  | 1        | 1127                 | 96922                      |
| AMALANCHA SPICATA  | 1        | 1127                 | 98049                      |
| AMALANCHA SPICATA  | 1        | 1127                 | 99176                      |
| AMALANCHA SPICATA  | 1        | 1127                 | 100303                     |
| AMALANCHA SPICATA  | 1        | 1127                 | 101430                     |
| AMALANCHA SPICATA  | 1        | 1127                 | 102557                     |
| AMALANCHA SPICATA  | 1        | 1127                 | 103684                     |
| AMALANCHA SPICATA  | 1        | 1127                 | 104811                     |
| AMALANCHA SPICATA  | 1        | 1127                 | 105938                     |
| AMALANCHA SPICATA  | 1        | 1127                 | 107065                     |
| AMALANCHA SPICATA  | 1        | 1127                 | 108192                     |
| AMALANCHA SPICATA  | 1        | 1127                 | 109319                     |
| AMALANCHA SPICATA  | 1        | 1127                 | 110446                     |
| AMALANCHA SPICATA  | 1        | 1127                 | 111573                     |
| AMALANCHA SPICATA  | 1        | 1127                 | 112700                     |

**GBH COMMERCIAL  
MITIGATION MONITORING PLAN**

The California Environmental Quality Act requires public agencies to report on and monitor measures adopted as part of the environmental review process (Section 21081.6, Public Resources Code [PRC]; Section 15097 of the CEQA Guidelines). This Mitigation Monitoring Plan (MMP) is designed to ensure that the measures identified in the Mitigated Negative Declaration are fully implemented. The MMP describes the actions that must take place as a part of each measure, the timing of these actions, the entity responsible for implementation, and the agency responsible for enforcing each action.

The City has the ultimate responsibility to oversee implementation of this Plan. The Community Development Director serves as the Project Monitor responsible for assigning monitoring actions to responsible agencies. Due to financial constraints, the City will require the applicant to fund a contract Project Monitor to undertake this effort. The commitment for this will be addressed in the Development Agreement and Conditions of Approval for the project.

As required by Section 21081.6 of the PRC, the Winters Community Development Department is the "custodian of documents and other material" which constitute the "record of proceedings" upon which a decision to approve the proposed project was based. Inquiries should be directed to:

Nelia Dyer, Community Development Director  
City of Winters  
530-795-4910 x 114

The location of this information is:

Winters City Hall  
Community Development Department  
318 First Street  
Winters, California 95694

In order to assist implementation of the mitigation measures, the MMP includes the following information:

**Mitigation Measure:** The mitigation measures are taken verbatim from the Negative Declaration.

**Timing/Milestone:** This section specifies the point by which the measure must be completed. Each action must take place during or prior to some part of the project development or approval.

**Responsibility for Oversight:** The City has responsibility for implementation of most mitigation measures. This section indicates which entity will oversee implementation of the measure, conduct the actual monitoring and reporting, and take corrective actions when a measure has not been properly implemented.

**Implementation of Mitigation Measure:** This section identifies how actions will be implemented and verified.

**Responsibility for Implementation:** This section identifies the entity that will undertake the required action.

**Check-off Date/Initials:** This verifies that each mitigation measure has been implemented.

Pursuant to Section 18.04.090 of the Winters Municipal Code related to the required CEQA Mitigation Monitoring Plan, sign-off on the completion of each mitigation measure in the adopted Mitigation

Monitoring Plan (MMP) shall constitute the required "Program Completion Certificate".

The Mitigation Monitoring Plan shall be adopted pursuant to the requirements of Section 18.04.060.A and implemented pursuant to Section 18.04.070.A - E, of the Winters Municipal Code.

The applicant shall fund the costs of implementing the MMP including the payment of fees specified in Section 18.04.100.A - D of the Winters Municipal Code.

Pursuant to Section 18.04.050 of the Winters Municipal Code related to the required CEQA Mitigation Monitoring Plan (MMP), the following items shall apply:

- The adopted MMP shall run with the real property that is the subject of the project and successive owners, heirs, and assigns of this real property are bound to comply with all of the requirements of the adopted Plan.
- Prior to any lease, sale, transfer, or conveyance of any portion of the real property that is the subject of the project, the applicant shall provide a copy of the adopted Plan to the prospective lessee, buyer, transferee, or one to whom the conveyance is made.
- The responsibilities of the applicant and of the City, and whether any professional expertise is required for completion or evaluation of any part of the Plan, shall be as specified in the Plan and as determined by the Community Development Director or designated Project Monitor in the course of administering the MMP.
- Cost estimates for the implementation of this Plan and satisfaction of each measure are not known or available, but shall be developed by the applicant in the course of implementing each mitigation measure.
- Civil remedies and criminal penalties for noncompliance with the adopted MMP are as specified in Sections 18.04.110 and 18.04.120 of the Winters Municipal Code.

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**Mitigation Measure Aesthetics 1** – Outdoor light fixtures shall be low-intensity, shielded and/or directed away from adjacent areas and the night sky. All light fixtures shall be installed and shielded in such a manner that no light rays are emitted from the fixture at angles above the horizontal plane. Lighting plans with certification that adjacent areas will not be adversely affected and that offsite illumination will not exceed 2-foot candles shall be submitted to the City for review and approval as part of improvement plans.

Prior to issuance of a building permit, the applicant shall submit a photometric and proposed lighting plan for the project to the satisfaction of the Community Development Department to ensure no spillover light and glare onto adjoining properties.

Timing/Milestone – Prior to issuance of a building permit.

Responsibility for Oversight – City of Winters.

Implementation of Mitigation Measure – Prior to issuance of a building permit for each phase or building, the applicant shall submit a photometric and proposed lighting plan to the satisfaction of the Community Development Department to ensure no spillover light and glare onto adjoining properties.

Responsibility for Implementation – Applicant and subsequent builders.

Check-off Date/Initials/Notes –

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### **Mitigation Measure Air 1**

- a. Construction equipment exhaust emissions shall not exceed District Rule 2-11 Visible Emission limitations.
- b. Construction equipment shall minimize idling time to 5 minutes or less.
- c. The prime contractor shall submit to the District a comprehensive inventory (i.e. make, model, year, emission rating) of all the heavy-duty off-road equipment (50 horsepower or greater) that will be used an aggregate of 40 or more hours for the construction project. District personnel, with assistance from the California Air Resources Board, will conduct initial Visible Emission Evaluations of all heavy-duty equipment on the inventory list.

An enforcement plan shall be established to weekly evaluate project-related on-and-off-road heavy-duty vehicle engine emission opacities, using standards as defined in California Code of Regulations, Title 13, Sections 2180 - 2194. An Environmental Coordinator, CARB-certified to perform Visible Emissions Evaluations (VEE), shall

routinely evaluate project related off-road and heavy duty on-road equipment emissions for compliance with this requirement. Operators of vehicles and equipment found to exceed opacity limits will be notified and the equipment must be repaired within 72 hours.

Construction contracts shall stipulate that at least 20% of the heavy-duty off-road equipment included in the inventory be powered by CARB certified off-road engines, as follows:

|                 |                        |
|-----------------|------------------------|
| 175 hp - 750 hp | 1996 and newer engines |
| 100 hp - 174 hp | 1997 and newer engines |
| 50 hp - 99 hp   | 1998 and newer engines |

In lieu of or in addition to this requirement, the applicant may use other measures to reduce particulate matter and nitrogen oxide emissions from project construction through the use of emulsified diesel fuel and or particulate matter traps. These alternative measures, if proposed, shall be developed in consultation with District staff.

Timing/Milestone – Prior to and during grading, and during appropriate period of construction.

Responsibility for Oversight – Yolo-Solano Air Quality Management District

Implementation of Mitigation Measure – The applicant shall satisfy the terms of the measure. Evidence of this shall be provided to the City.

Responsibility for Implementation – Applicant

Check-off Date/Initials/Notes –

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### Mitigation Measure Air 2

- a) Nontoxic soil stabilizers according to manufacturer's specifications shall be applied to all inactive construction areas (previously graded areas inactive for ten days or more).
- b) Ground cover shall be reestablished in disturbed areas quickly.
- c) Active construction sites shall be watered at least three times daily to avoid visible dust plumes.
- d) Paving, applying water three times daily, or applying (non-toxic) soil stabilizers shall occur on all unpaved access roads, parking areas and staging areas at construction sites
- e) Enclosing, covering, watering daily, or applying non-toxic soil binders to exposed stockpiles (dirt, sand, etc.) shall occur.

- f) A speed limit of 15 MPH for equipment and vehicles operated on unpaved areas shall be enforced.
- g) All vehicles hauling dirt, sand, soil, or other loose materials shall be covered or shall be maintained at least two feet of freeboard.
- h) Streets shall be swept at the end of the day if visible soil material is carried onto adjacent public paved roads.

**Timing/Milestone** – Prior to and during grading, and during appropriate period of construction.

**Responsibility for Oversight** – Yolo-Solano Air Quality Management District

**Implementation of Mitigation Measure** – The applicant shall satisfy the terms of the measure. Evidence of this shall be provided to the City.

**Responsibility for Implementation** – Applicant

**Check-off Date/Initials/Notes** –

**Mitigation Measure Biological 1** – The project proponent shall mitigate for potential project-related impacts to burrowing owl by conducting a pre-construction survey no more than 30 days prior to the initiation of construction activity. The pre-construction survey shall be conducted by a qualified biologist familiar with the identification of burrowing owls and the signs of burrowing owl activity. If active burrows are found on the project site, the California Department of Fish and Game (CDFG) shall be consulted regarding appropriate mitigation measures for project-related impacts to burrowing owl. Pursuant to the CDFG document entitled "Staff Report on Burrowing Owl Mitigation" (September 25, 1995), it is likely that replacement habitat will be required by CDFG. The guidelines include specific mitigation to protect nesting and wintering owls and to compensate for loss of breeding sites. In general, if the project would remove habitat of an occupied breeding site (e.g., if an active nest and surrounding habitat are removed), the project proponent will be required to compensate by preserving equivalent suitable habitat for each active nest site. In addition, the project proponent must install artificial burrows to offset the direct loss of the breeding site. Mitigation shall be consistent with the City's adopted Habitat Mitigation Program. Implementation of this mitigation measure shall be confirmed by the City of Winters prior to the initiation of construction activity.

**Timing/Milestone** – Not more than 30 days prior to commencement of grading or any physical modification of undeveloped portions of the site.

**Responsibility for Oversight** – City of Winters

**Implementation of Mitigation Measure** – The applicant shall coordinate with the appropriate agency(s) to satisfy the terms of the measure. Evidence of this shall be provided to the City. The survey shall be performed by a qualified biologist in accordance with accepted protocols.

**Responsibility for Implementation – Applicant**

**Check-off Date/Initials/Notes –**

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**Mitigation Measure Biological 2** – The project proponent shall mitigate for potential project-related impacts to nesting raptors (Swainson's Hawk, White-tailed Kite, Northern Harrier, and Loggerhead Shrike) by conducting a pre-construction survey of all trees suitable for use by nesting raptors on the subject property or within 0.25 mile of the project boundary as allowable. The preconstruction survey shall be performed no more than 30 days prior to the implementation of construction activities. The preconstruction survey shall be conducted by a qualified biologist familiar with the identification of raptors known to occur in the vicinity of the City of Winters. If active special-status raptor nests are found during the preconstruction survey, a 0.25-mile (1,320-foot) buffer zone shall be established around the nest and no construction activity shall be conducted within this zone during the raptor nesting season. The buffer zone shall be marked with flagging, construction lathe, or other means to mark the boundary of the buffer zone. All construction personnel shall be notified as to the existence of the buffer zone and to avoid entering the buffer zone during the nesting season. Implementation of this mitigation measure shall be confirmed by the City of Winters prior to the initiation of construction activity.

**Timing/Milestone** – Not more than 30 days prior to commencement of grading or any physical modification of the site.

**Responsibility for Oversight – City of Winters**

**Implementation of Mitigation Measure** – The applicant shall coordinate with the appropriate agency(s) to satisfy the terms of the measure. Evidence of this shall be provided to the City. The survey shall be performed by a qualified biologist in accordance with accepted protocols.

**Responsibility for Implementation – Applicant**

**Check-off Date/Initials/Notes –**

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**Mitigation Measure Biological 3** – The project proponent shall mitigate for potential project-related impacts to migratory birds by conducting a pre-construction survey for nests on the site. The preconstruction survey shall be performed no more than 14 days prior to the onset of vegetation and/or

tree removal. The preconstruction survey shall be conducted by a qualified biologist familiar with the identification of migratory bird known to occur in the vicinity of the City of Winters. If active migratory bird nest(s) are found onsite during the preconstruction survey, the nest(s) shall not be disturbed or removed until the young have fledged and the nest is no longer active. A buffer may be required. All construction personnel shall be notified as to the existence of the buffer zone and to avoid entering the buffer zone during the nesting season. Implementation of this mitigation measure shall be confirmed by the City of Winters prior to the initiation of construction activity.

Alternatively, potential impacts to nesting birds or unfledged young would be avoided if vegetation and/or tree removal occurred only between September 1 and January 21.

Timing/Milestone – Not more than 14 days prior to commencement of grading or any physical modification of the site.

Responsibility for Oversight – City of Winters

Implementation of Mitigation Measure – The applicant shall coordinate with the appropriate agency(s) to satisfy the terms of the measure. Evidence of this shall be provided to the City. The survey shall be performed by a qualified biologist in accordance with accepted protocols.

Responsibility for Implementation – Applicant

Check-off Date/Initials/Notes –

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**Mitigation Measure Biological 4** – Any mitigation required shall be implemented in a manner consistent with requirements, purpose and intent of the City of Winters' Habitat Mitigation Program.

Timing/Milestone – Prior to commencement of grading or any physical modification of the site.

Responsibility for Oversight – City of Winters

Implementation of Mitigation Measure – The applicant shall coordinate with the appropriate agency(s) to satisfy the terms of the measure. Evidence of this shall be provided to the City.

Responsibility for Implementation – Applicant

Check-off Date/Initials/Notes –

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**Mitigation Measure Cultural 1** – If cultural resources (historic, archeological, paleontological, and/or human remains) are encountered during construction, workers shall not alter the materials or their context until an appropriately trained cultural resource consultant has evaluated the situation. Project personnel shall not collect cultural resources. Prehistoric resources include chert or obsidian flakes, projectile points, mortars, pestles, dark friable soil containing shell and bone dietary debris, heat-affected rock, or human burials. Historic resources include stone or adobe foundations or walls, structures and remains with square nails, and refuse deposits often in old wells and privies.

Timing/Milestone – During grading, construction of infrastructure, and construction of each building.

Responsibility for Oversight – City of Winters; Yolo County Coroner; State Native American Heritage Commission.

Implementation of Mitigation Measure – If other archeological or cultural resources are found, all grading and activity in the immediate area shall cease, the finds shall be left in place, and the project archeologist and the Community Development Department (530) 795-4910 x 114 shall be contacted to assess the find and determine how to proceed.

Responsibility for Implementation – Applicant

Check-off Date/Initials/Notes –

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**Mitigation Measure Cultural 2** - Should human remains be discovered, no further site disturbance shall occur until the county coroner has determined that the remains are not subject to the provisions of Section 27491 of the Government Code or any other related provisions of law concerning investigation of the circumstances, manner and cause of any death, and the recommendations concerning the treatment and disposition of the human remains have been made to the person responsible for the excavation, in the manner provided in Section 5097.98 of the Public Resources Code. If the coroner determines that the remains are not subject to his or her authority and the remains are recognized to be those of a Native American, the coroner shall contact the Native American Heritage Commission within 24 hours.

Timing/Milestone – During grading, construction of infrastructure, and construction of each building.

Responsibility for Oversight – City of Winters; Yolo County Coroner; State Native American Heritage Commission.

Implementation of Mitigation Measure – If human remains are found, all grading and activity in the immediate area shall cease, the find shall be left in place, and the applicant shall immediately notify the Yolo County Coroner at (530) 666-8282, the Community Development Department at (530) 795-4910 x114, and the project archeologist to assess the find and determine how to proceed. If the remains are

found to be of Native American descent, the Native American Heritage Commission shall also be notified at (916) 653-4082, pursuant to the terms of the measure.

Responsibility for Implementation – Applicant

Check-off Date/Initials/Notes –

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**Mitigation Measure Geology 1** – The applicant shall submit a soils and geotechnical report upon submittal of the initial improvement plans package. The improvement plans shall be signed by the soils engineer for conformance to the geotechnical report prior to approval by the City.

Timing/Milestones – Submittal of initial improvement plans package.

Responsibility for Oversight – City of Winters

Implementation of Mitigation Measure – This shall be documented on each set of building plans and verified during plan check.

Check-off Date/Initials/Notes –

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**Mitigation Measure Land Use 1** – All aspects of the project shall be subject to design review to ensure compatibility with the surrounding area and satisfaction of the Community Design Guidelines and other applicable principles of good community design.

Timing/Milestone – Prior to issuance of a building permit for each phase of construction of the project, the applicant shall submit full architectural renderings, including building elevations and floor plans, for design review and approval.

Responsibility for Oversight – City of Winters

Implementation of Mitigation Measure – Per the terms of the measure.

Responsibility for Implementation – Applicant

Check-off Date/Initials/Notes –

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**Mitigation Measure Land Use 2** – The proposed project height and parking provisions are subject to approval of a Planned Development Overlay for the subject property.

**Timing/Milestone** – Prior to issuance of a building permit for each phase of construction of the project.

**Responsibility for Oversight** – City of Winters

**Implementation of Mitigation Measure** – Per the terms of the measure.

**Responsibility for Implementation** – Applicant

**Check-off Date/Initials/Notes** –

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**Mitigation Measure Noise 1** – The project applicant shall submit a construction noise mitigation plan to the City of Winters for review and approval. The plan shall depict the location of construction equipment and describe how noise would be mitigated through methods such as, but not limited to, locating stationary noise-generating equipment (such as pumps and generators) as far as possible from nearby noise-sensitive receptors. Where practicable, noise-generating equipment will be shielded from nearby noise-sensitive receptors by noise-attenuating buffers such as structures or haul trucks. Onsite noise sources such as heavy equipment located less than 200 feet from noise-sensitive receptors will be equipped with noise-reducing engine housings. Portable acoustic barriers able to attenuate at least 6 dB will be placed around noise-generating equipment located within 200 feet of existing residences. Water tanks and equipment storage, staging, and warm-up areas shall be located as far from noise-sensitive receptors as possible. All noise attenuation measures identified in the plan shall be incorporated into the project.

**Timing/Milestone** – Prior to approval of improvement plans.

**Responsibility for Oversight** – City of Winters

**Implementation of Mitigation Measure** – During grading, construction of infrastructure, and construction of each building.

**Responsibility for Implementation** – Applicant

**Check-off Date/Initials/Notes** –

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**Mitigation Measure Noise 2** – Construction activities shall adhere to the following noise requirements:

All construction equipment shall utilize noise reduction features (e.g., mufflers and engine shrouds) that are no less effective than those originally installed by the manufacturer.

Hours of construction shall comply with those established in Chapter 8.20.100 of the Winters Municipal Code. Those hours are weekdays from 7:00 a.m. through 7:00 p.m. Construction is prohibited on weekends and federal holidays.

**Timing/Milestone** – Site improvement and construction

**Responsibility for Oversight** – City of Winters

**Implementation of Mitigation Measure** – During grading, construction of infrastructure, and construction of each building.

**Responsibility for Implementation** – Applicant

**Check-off Date/Initials/Notes** –

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**Mitigation Measure Utilities 1** – The proposed systems for conveying project sewage, water, and drainage shall be finalized and approved by the City Engineer prior to approval of improvement plans. The project is required to fund and construct off-site improvements necessary to support the development. Such improvements could include, but not be limited to a water well, water lines, sewer lines and storm drainage lines. Should property acquisition or additional CEQA clearance be required for off-site improvements, this will be the responsibility of the developer.

Timing/Milestone – Prior to approval of improvement plans.

Responsibility for Oversight – City of Winters

Implementation of Mitigation Measure – As specified in the measure.

Responsibility for Implementation – Applicant

Check-off Date/Initials/Notes –

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**Mitigation Measure Utilities 2** – Building permits shall be issued for each building only after the City Engineer has established that water supply will be available to serve the building.

Timing/Milestone – Prior to issuance of building permits.

Responsibility for Oversight – City of Winters

Implementation of Mitigation Measure – As specified in the measure.

Responsibility for Implementation – Applicant

Check-off Date/Initials/Notes –



**PLANNING COMMISSION  
STAFF REPORT**

**TO:** Honorable Chair and Commissioners  
**DATE:** October 30, 2012  
**FROM:** John W. Donlevy, Jr., City Manager *JWD*  
**SUBJECT:** Approval of Alley Activation Plan, an Addendum to the Winters Downtown Master Plan

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**RECOMMENDATION:** That the Planning Commission approve the Alley Activation Visioning Plan, an Addendum to the Winters Downtown Master Plan.

**BACKGROUND:** In December of 2010, City staff submitted an application to the Local Government Commission (LGC) for a \$15,000 grant for technical assistance in the development of a visioning plan for the activation of the downtown alley known as "Newt's Expressway." The application was approved in late spring, 2011 and Terry Bottomley of Bottomley Design and Planning was contracted with to facilitate stakeholder and community meetings and prepare graphics.

The visioning plan is an addendum to the Downtown Master Plan and includes design concepts for pedestrian and bicycle improvements, beautification features, outdoor spaces and seating. The activation of the alleyway will allow for an attractive expansion of the downtown core area. Several comments were received at the stakeholder and community meetings and they are incorporated into the visioning plan. The visioning plan is conceptual in nature, actual design plans including build out details will come before the Planning Commission through the regular approval process thus allowing for further public comment.

**FISCAL IMPACT:** None by this action.

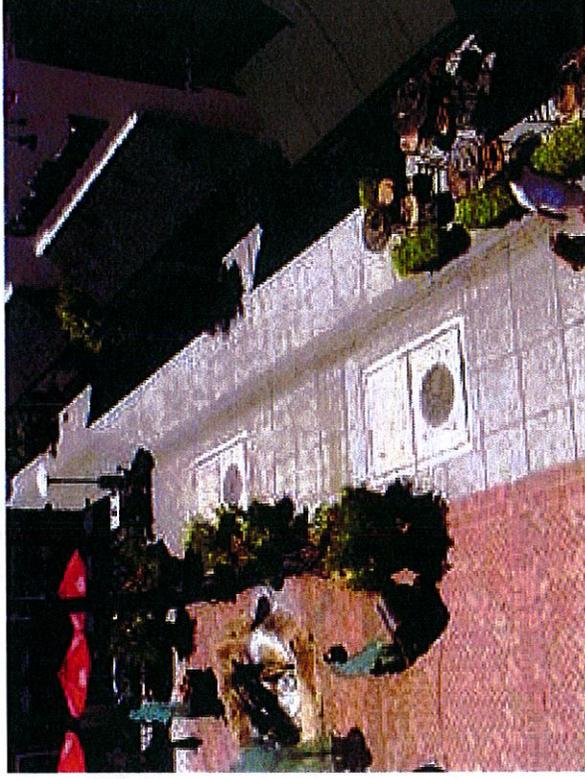
**ATTACHMENTS:** Alley Activation Visioning Plan, an Addendum to the Winters Downtown Master Plan



October 2012

**CITY OF WINTERS**  
**ALLEY ACTIVATION VISIONING PLAN**  
**an Addendum to the Downtown Master Plan**

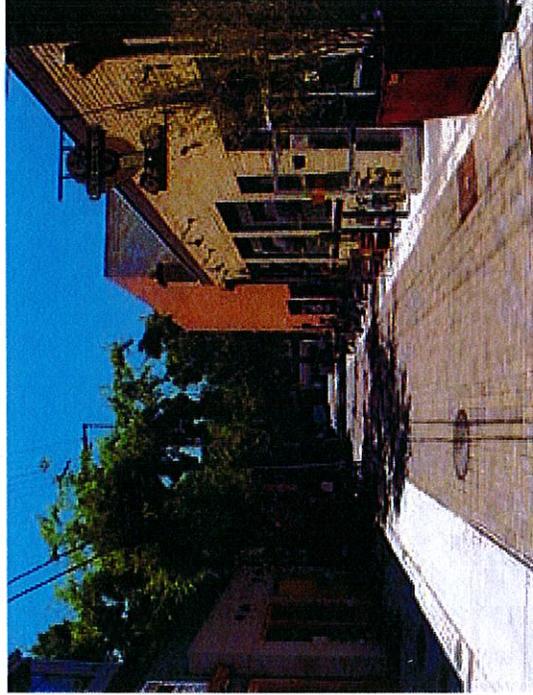
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## I. Foreword

In late Spring of 2011 the City of Winters was awarded a grant from the Local Government Commission (LGC) for technical assistance in the development of a visioning plan for the activation of the alley known as “Newt’s Expressway” located between Railroad Avenue, First, Main and Abbey Streets. This visioning plan will serve as an addendum to the City of Winters Downtown Master Plan.



Sacramento-Alley between L and Capitol, 17th and 18th

## II. Visioning Plan

Creation of conceptual design ideas to enhance connectivity by providing pedestrian and bicycle improvements and to develop outdoor spaces and seating in the Core Block A alley, including a mid-block paseo between Main and Abbey Streets.

Activating the alley in a manner consistent with the Historic District to provide for vibrant public space and an extension of the core downtown area.

## III. Today’s Situation

Uneven, cracked pavement; overgrown weeds; unconcealed dumpsters; overhead utilities; poor lighting





#### **IV. Vision: Clean-Up/Fix-Up**

1. Enclose dumpsters using complementary materials
2. Replace old paving, pervious as feasible
3. Upgrade fencing materials
4. Rear building paint-up/fix-up
5. Add landscape materials, potted or permanent
6. Formalize/pave parking areas

#### **V. Vision: Reuse with Leasable Area**

1. Building renovation
2. Replace temporary structure(s) and parking with usable / leasable space
3. Enclose dumpsters, use complementary materials
4. Add landscape materials, potted or permanent
5. Provide area lighting, signage, bike parking

#### **VI. Vision: New Infill Construction**

1. New rear building addition / construction
2. Incorporate signage, lighting per downtown guidelines
3. Add landscape materials, consider greenwalls, etc
4. Provide seating, bike parking, amenities



#### **VII. Recommendations**

##### Guidelines for Private Site and Building Renovations

1. Improvements will conform with Form Based Code criteria
2. Use of way finding signage
3. Establishment of a commercial corridor (utilizing both sides of buildings)

##### Trash Management

1. Consideration of compactors
2. Site/building locations - architectural dumpsters
3. Common area(s) sites

##### Public Area Improvements

1. Alley paving and lighting



2. Paseo improvements program - art park, water conservation display, water feature, paving improvements
3. Utility undergrounding
4. Pedestrian link
5. Public spaces
6. Amenities - bicycle racks, seating, landscaping, etc.

